



COLUMBIA HEIGHTS

Public Realm Framework

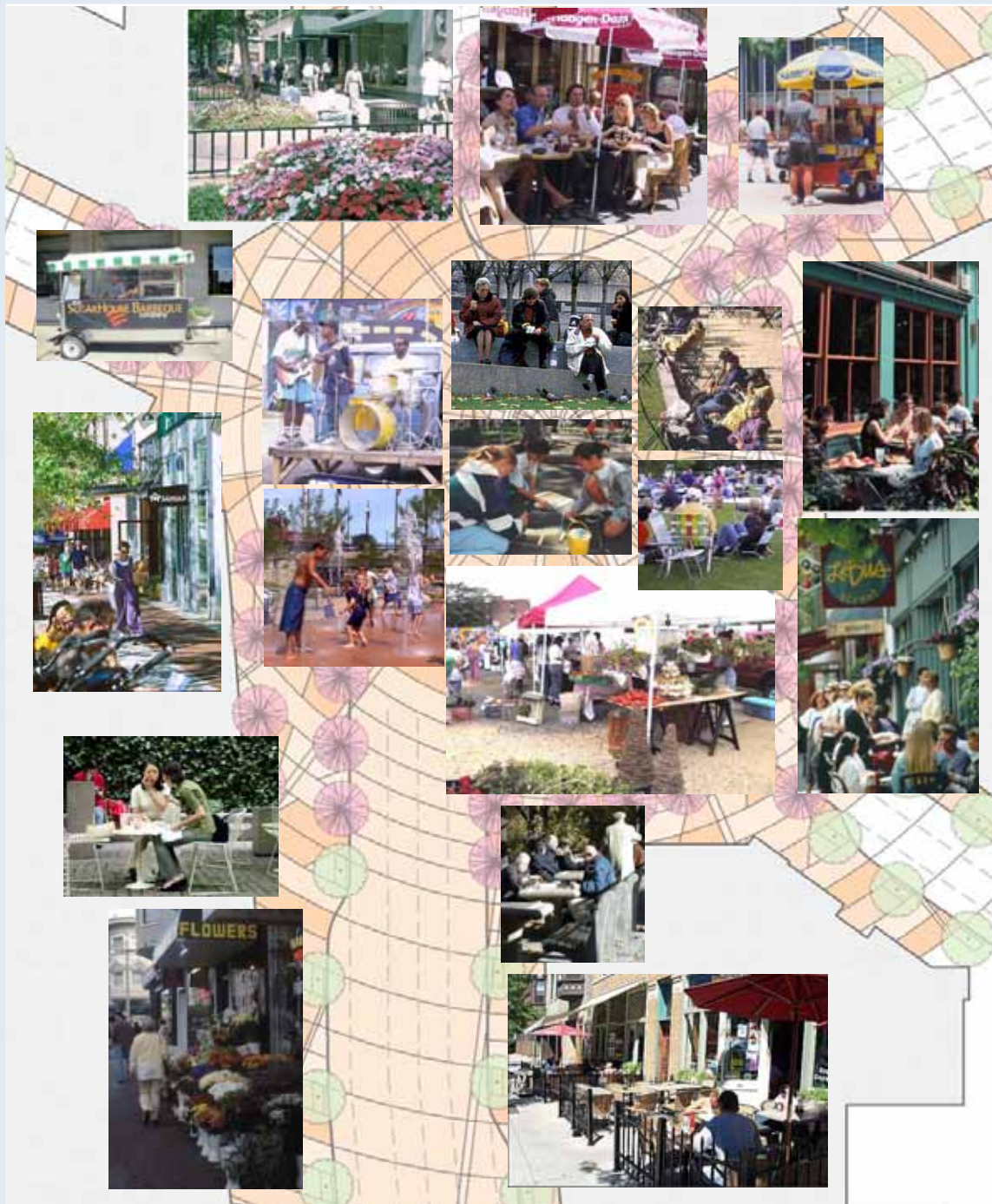


Government of the District of Columbia
Office of Planning
October 2004

Executive Summary



The Public Realm Framework and the Vision for the Civic Plaza (below) has been Guided by the Aspirations of the Community.





14th Street Commercial Corridor, 1940s
Source: 1997 Community-Based Plan, WAF



The Neighborhood's identity is represented in its art

The Neighborhood

Background

In 1997, concerned residents and stakeholders in Columbia Heights participated in a series of community workshops to create a redevelopment strategy for the neighborhood called "A Community-Based Plan for the Columbia Heights Metro Station Area". The strategy identified and proposed redevelopment solutions for several major parcels, concentrating on the 14th Street commercial corridor. These solutions included:

- 'mixed-use' development in the neighborhood 'core area', along the 14th Street corridor, in relation to the development of the new Columbia Heights Metro Station,
- development of a new Civic Plaza on a publicly-owned parcel at the intersection of Park Road, Kenyon Street, and 14th Street,
- development of high quality public streetscape improvements along 14th Street, connecting to the new Metro station.

The strategy also established a list of 'Planning Principles' to guide and inform future development within the neighborhood.

Since the plan was completed, the Columbia Heights area has been the focus of a major neighborhood redevelopment effort by the District government and other public entities, including the National Capital Revitalization Corporation (NCRC), the RLA Revitalization Corporation (RLA), and the Washington Metropolitan Area Transit Authority (WMATA). In 1999, WMATA opened the Columbia Heights Metro Station. Since then, RLA and NCRC have been responsible for awarding redevelopment rights for many parcels within the Columbia Heights neighborhood 'core area', along the 14th Street corridor.

The DC Office of Planning (DC-OP) initiated this planning project in early 2003, to coordinate high quality 'Public Realm' improvements -- those within the public domain of civic space and public streetscape -- to foster a unified community identity for Columbia Heights.

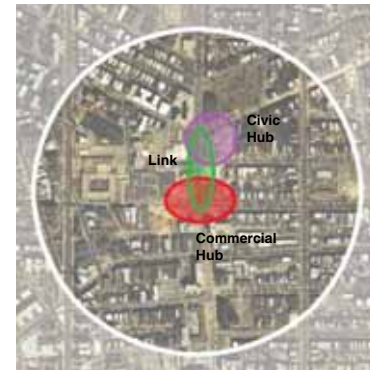
Planning Goals

The following 'Project Goals' were developed by DC-OP and the Design Team, endorsed by the Advisory Committee, and presented and adopted during the first community meeting.

- To understand the cultural and physical background and evolving history of the Columbia Heights Neighborhood.
- To create an urban design 'Framework Plan' for the Public Realm, to define public places and streetscapes, which integrate public space with private and public development.
- To create streetscapes which define a 'Community Design identity', which captures the unique qualities of Columbia Heights.
- To define design guidelines and design development documents for the Public Realm, consistent with the Planning Principles of the 1997 Community-Based Plan.
- To develop and recommend implementation strategies for the Plan, which coordinate public and private investment.



The location of Columbia Heights neighborhood within the District



A Community Based Plan for the Columbia Heights Metro Station Area, 1997



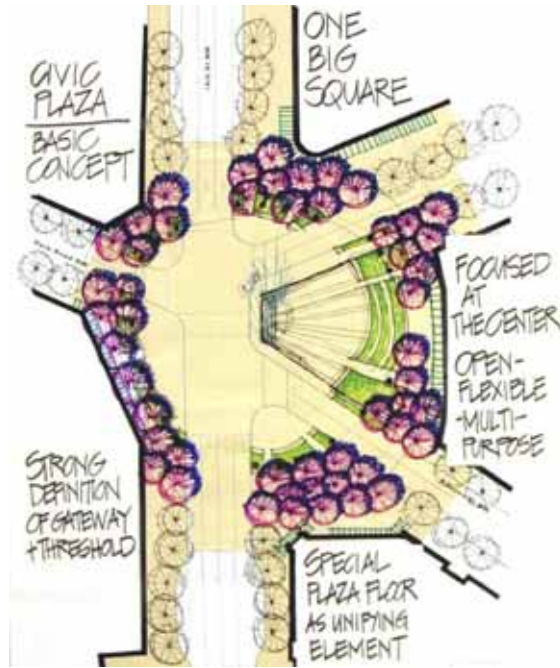
Redevelopment Activity
(RLARC Development Parcels identified in yellow)

Parcel Number	Project
• Parcel 24	Dance Institute
• Parcel 29	Tivoli Partners
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• Metro	Triangle, II
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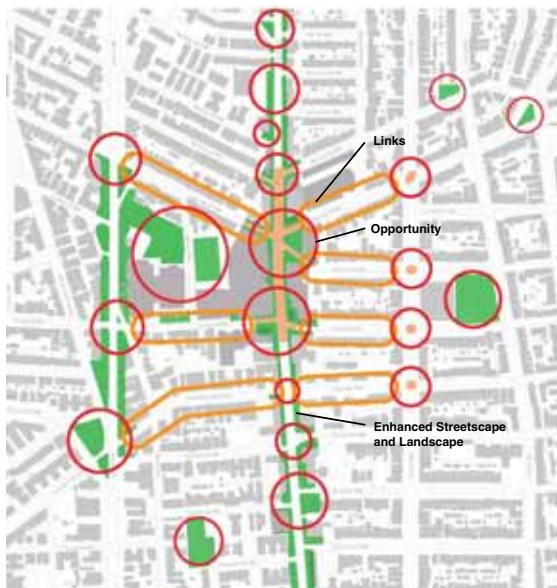
COLUMBIA HEIGHTS



The Framework Design Concept



The Civic Plaza Design Concept



Neighborhood Design Opportunities

Public Realm Framework Goals

The following goals guided the development of the Framework:

1. Strengthen Community Identity
2. Celebrate Diversity
3. Create a Lively Urban Experience
4. Make it Easy to Walk
5. Make Public Transit Convenient
6. Make it Safe.

Neighborhood Characteristics

Community residents identified the following:

- a commitment to embrace the extraordinary cultural and economic diversity,
- a sense of enthusiasm and vitality for the public realm,
- a spirit of openness and tolerance,
- an interest in promoting development which respects its past while looking forward to the future.

Design Concept

The Public Realm Framework Design Concept illustrates the urban design strategy for Columbia Heights. The Concept:

- focuses on the neighborhood "core area" -- the "Civic/Cultural" and "Commercial Heart" -- extending along 14th Street from Columbia Road to Monroe Street,
- creates a new Civic Plaza, at the intersection of Park Road, Kenyon Street and 14th Street,
- enhances the connection between the Civic Plaza, the Metro Station plazas and adjacent development,
- integrates public realm and private development,
- captures community identity,
- identifies a palette of materials which can be replicated in symbolic ways at several key intersections and gateways throughout the neighborhood.

1. Cultural multi-cultural humanity multi-generational 'kaleido-cultural'	multi-economic community oriented mix of faces neighborhood	melting-pot multi-ethnic familiar faces multi-lingual
2. Ambience / Atmospheric Vital sense of conflict urban uncut/raw chaos	'a lot going on' unrefined crowds vitality 'spike lee joint'	crossroads vibrancy edgy-tension density skyline
3. Social open colors accommodating flexible contrasts multi-generational	tolerance multi-purpose/use old/new accepting non-linear	'kaleidoscope' welcoming shapes diverse rooftops
4. Temporal / Changing changing emerging	evolving revival	history/historic 'renaissance'
5. Humanitarian soulful we/the people as 'art'	personal people-focused (not object-focused)	intimate

The words used by residents to describe their neighborhood



The stepped lawn will provide an opportunity to watch neighborhood activity



A flowering, ornamental tree will ring the plaza



The interactive fountain will provide a dynamic place for children to play and relief from summertime heat



The sculptural 'flowers' will provide shading and ornamental lighting

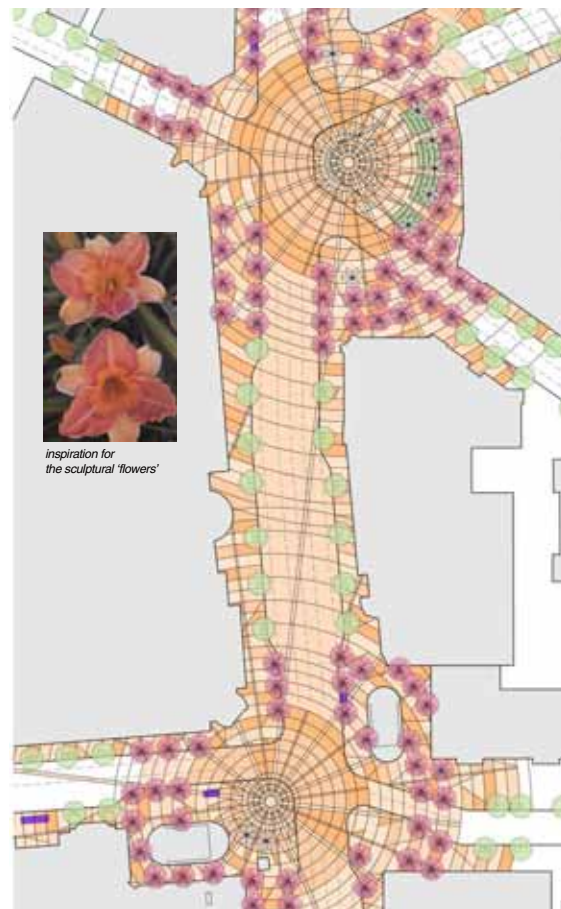


The Civic Plaza

The Design of the Civic Plaza and Metro Plazas

The Plaza is located in the triangular shaped parcel at the intersection of Kenyon St., Park Road, and 14th Street. It is approximately 12,000 square feet in area. The design theme is based upon a 'kaleidoscope' which symbolizes and embodies the rich diversity of the community.

- The Plaza is organized around a strong geometric paving pattern, derived from overlaying a kaleidoscopic image upon a radial-concentric design which emanates from the center.
- The paving extends across the intersection, unifying the entire open space and new buildings, to create the sense of a much larger and cohesive public space. Thus, the plaza can comprise the small triangular park; or include the sidewalks across Kenyon St. and Park Road; or become the entire space encompassed in the pattern. Occasionally, streets may be closed and the entire space may be used to host community events. The triangular plaza can support 800 - 1000 people, while the entire, expanded plaza space can support up to 3,000 people for special community events.
- The Plaza is predominantly 'hardscaped' and is designed to be flexible enough to accommodate a wide range of uses.
- A special circle, thirty feet in diameter is located at the center of the Plaza. Here, the Plaza forms a shallow bowl and is depressed approximately 12 inches and surrounded by seat walls on three sides. Located within this circle is the primary feature of the Plaza – a public, interactive fountain with informally placed jets of water that may be programmed in a syncopated rhythm. The fountain symbolizes the spontaneity and vitality of the Columbia Heights neighborhood. It is also designed to have several 'phases of intensity', which facilitate diverse uses of the Plaza.
- The 'hardscape' is softened by the use of a terraced lawn -- a series of grassy steps which also provide an opportunity for seating and relaxation. The steps are five feet wide, terrace upwards away from the plaza center in low steps, and are framed in low concrete walls which function as seat walls to the adjacent sidewalk cafes.
- The entire edge of the Plaza is defined by planted rings of flowering ornamental trees which surround the east side of the lawn, and extend across Park Road, Kenyon Street and 14th Street.
- The edge of the Plaza is further defined by a ring of large-scale sculptural 'flowers', which provide shade canopies and ornamental lighting within the area of the lawn, and serve as formal 'signature' elements. These flowers are designed in the form of day lilies and constructed of metal 'stems' with 'petals' constructed of a tensile fabric over light-weight metal frames. The ring also extends across Park Road and Kenyon Street, where single flowers are located to mark those portions of the extended Plaza.
- The radial-concentric pattern extends down 14th Street to connect with the Metro Station entry plazas. A radial element from the Civic Plaza connects to a smaller second 'center' of the pattern at the Metro entries. The 14th St. / Irving St. intersection is also ringed with ornamental flowering trees to mark the gateway into the 'core area' from Metro-rail. Additional 'flower' elements also are placed at the Metro plazas to strengthen the relationship with the Civic Plaza.

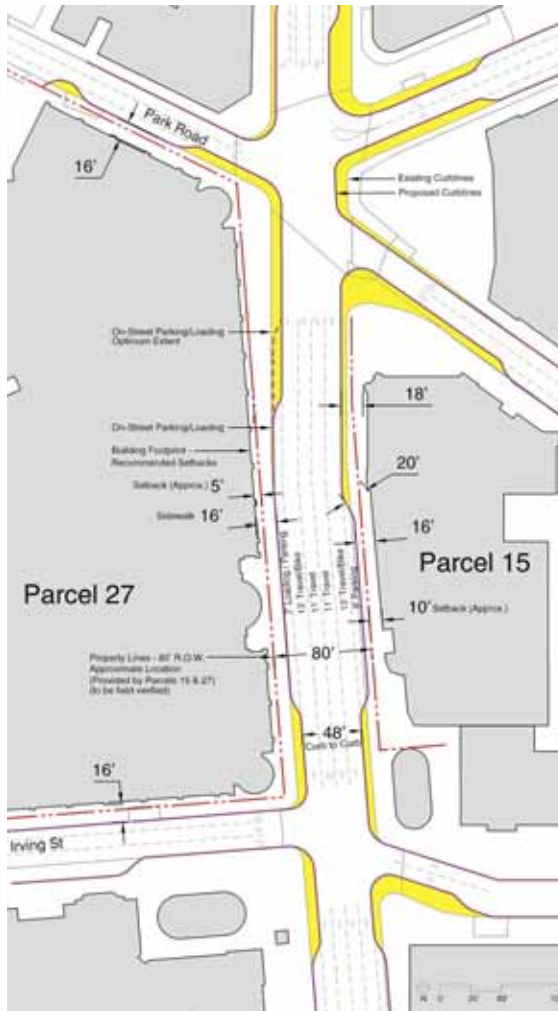


inspiration for the sculptural 'flowers'

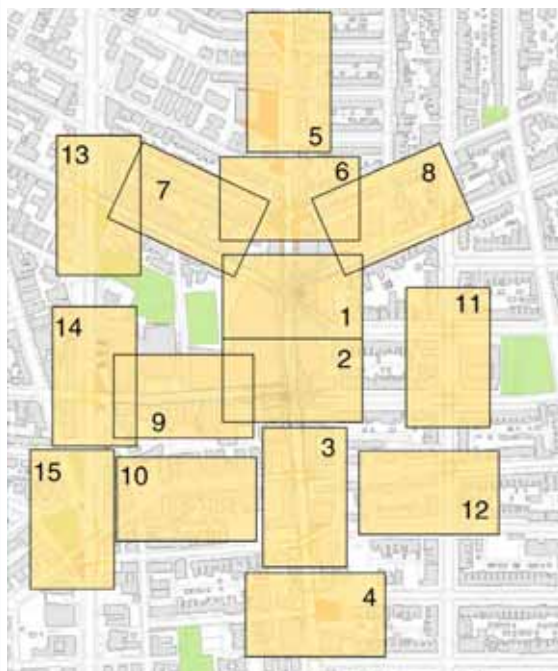
The unique paving pattern, together with the sculptural 'flowers', ornamental trees, and the integration of public art are intended to establish and reinforce a special civic place and community identity for the core of Columbia Heights.



The Metro Station Plazas



14th Street Right-of-Way: The areas in yellow identify curb extensions



Focus Area Design Plans



Traditional Lighting



Custom Bus Shelters

14th Street Right-of-Way Design

Within the 'core area', 14th street should be reconfigured to establish balanced access for all modes to and around the Metro Station entrances and to support traffic patterns which complement the neighborhood's commercial core. The reconfiguration of travel lanes will also provide a gradual transition from four travel lanes south of Irving Street to the existing two travel lanes north of the Monroe Street.

14th Street, between Irving Street and the Park/Kenyon intersection, should accommodate two 11 foot wide travel lanes in the center, two 14 foot wide combined travel-bike lanes at the outside, and on-street parking lanes along both sides of the street. This allows the connection of bike lanes to the Metro Plazas and on-street parking which reinforces the commercial uses along the street and enhances the pedestrian environment. Sidewalks must be a minimum 16 feet wide in the 'core area' to accommodate a relatively generous through-pedestrian zone and a furnishing zone along the curb. Door swings, cafe tables, and all utilities, should be held back from the 16 foot dimension. Where possible, a 20 foot wide sidewalk is recommended which would allow a four foot zone adjacent to the building to be used by the businesses to "enliven" their storefronts with tables or displays. Setbacks of adjacent properties may be required to provide the desired sidewalk widths.

Each of the intersections along 14th Street, from Monroe Street to Girard Street, should be reconfigured with generous curb extensions to enhance pedestrian safety and strengthen the public realm. The Transportation Study includes additional recommendations for improving pedestrian safety such as modifications to traffic signalization.

- The Monroe Street / 14th Street intersection should be reconfigured to provide better and safer access along Monroe across 14th Street.
- 14th Street from Monroe Street to Park Road should include two travel lanes, two dedicated bike lanes, and on-street parking lanes on both sides of the street. South-bound at Park Road, the parking lane converts to a dedicated right-turn lane.
- The Kenyon Street / Park Road / 14th Street intersection should be reconfigured to improve pedestrian safety. West-bound traffic at the Park Road / 14th Street intersection will permit a right-turn only northbound onto 14th Street. Kenyon Street will be reduced to two travel lanes at 14th Street.
- North-bound travel on 14th Street at the Park Road / Kenyon Street intersection will have a dedicated left-turn lane and one through travel lane which extends north to Monroe.
- The Irving Street / 14th Street intersection is also improved by narrowing the roadbed to provide a south-bound left turn lane and one through travel lane.

Focus Area Design Plans

The Focus Area Design Plans detail recommendations for specific areas throughout the neighborhood including special intersections and opportunity areas, street trees, lighting and sidewalks. The Plans are organized by priority. First priority areas include Plans 1 through 8. Second priority areas are Plans 9 through 12. Third priority areas are Plans 13 through 15.



High quality streetscapes and adjacent uses will enhance the pedestrian environment

Streetscape Design

The streetscape design for Columbia Heights is derived from the design of the core area. Streetscape elements include the three different scales of the standard DC ornamental street light poles placed at slightly more dense spacing in the core area to emphasize its importance.

Street trees along 14th Street will be London Plane or Linden trees, which have a slightly vertical form, and will provide a generous canopy for shading along the sidewalk through the commercial center of Columbia Heights. The plane trees will be placed at 40 foot intervals along 14th Street, within curbside planters filled with hearty ground-cover plantings and edged with DC standard iron hoops.

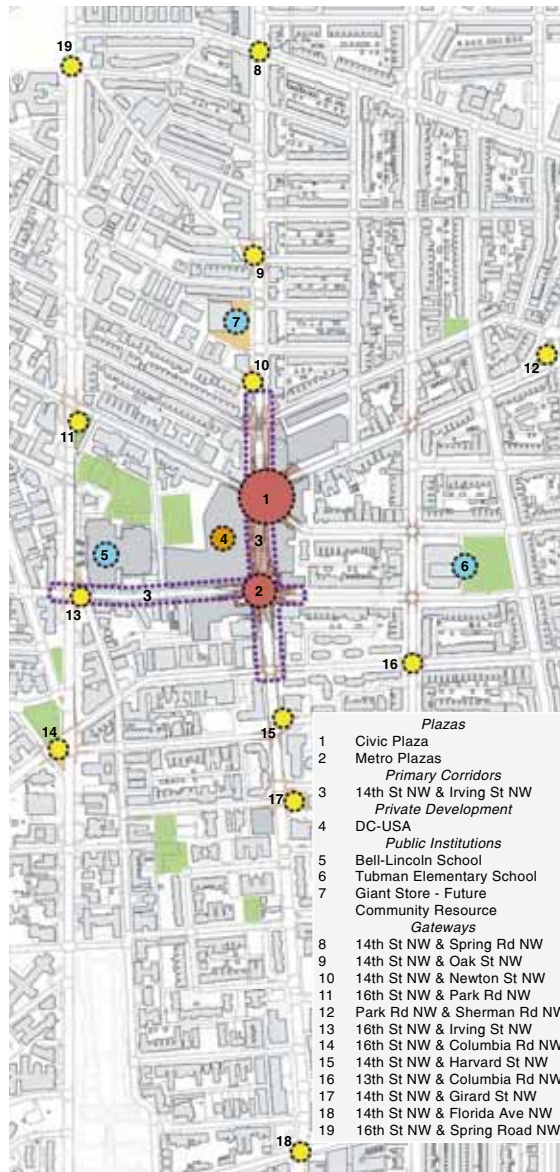
Within the core area, the sidewalks are proposed to be at least 16 feet in width to provide space for a generous pedestrian zone, and for a furnishing zone along the street edge for street trees, ornamental street lights, and occasional benches, planters and bus shelters.

Public Art Framework

The DC Commission on the Arts and Humanities supported the selection of two artists to work with the Design Team on integrating public art concepts into the Public Realm. A public art framework was developed to identify opportunities in the community for public art installations. In addition, several conceptual plans for public art were created. These include: enhancements to paving and seating, custom designed street furnishings for lighting, shading and community history.



Potential Public Art Mosaics



The Public Art Framework



Potential Art Screens - Community Portraits and Masks



Preliminary Construction Budget Estimate Core Area Site Plan

Preliminary Construction Budget Estimates for Public Realm Elements

The construction costs for public realm improvements within the 'core area' have been developed in consultation with the DC Office of Planning, the District Department of Transportation and the Department of Public Works. They are based upon relevant construction costs for similar public right-of-way projects.

Construction costs for public improvements within the 'core area' are estimated at approximately \$6.0 to \$6.4 million. Consistent with conventional practice for costs planned at this stage of project development, these estimates include a 20% design/construction contingency.

The construction costs itemize the public right-of-way costs in relation to each adjoining development parcel, and are based upon area calculations for the right-of-way. Total project costs should be budgeted upon existing standard DC DDOT and DPW practices. Total project costs are estimated at \$7.2 to \$8 million, 20-25% higher than base construction costs.

Public Realm Phasing Projected Construction Schedule

Phasing Diagram LD	RLA Parcels	Project Name	2003				2004				2005				2006			
			Su	Fa	Wi	Sp	Su	Fa	Wi	Sp	Su	Fa	Wi	Sp	Su	Fa	Wi	Sp
Curbside Improvements Development Projects																		
A	29	Tivoli Theater																
B	Metro	Triangle II																
C	38	Civic Plaza																
D	15	Donatelli & Klein																
	26	Donatelli & Klein																
E	27	DC/USA																
F	24	Dance Institute																
Curbside Improvements - Adjacent to Existing Development																		
A1		Riggs																
D1		CVS																
F1		NE 14th St																
Streetside Improvements - 14th Street NW																		
G1		14th / Monroe																
G2		14th / Park / Kenyon																
G3		14th Midblock																
G4		14th / Irving																
Improvements Adjacent to Core Area																		
H		Bell Lincoln School																
J		Irving St																
K		Park Road																
Projected Building Construction Schedule																		
Potential Public Realm Construction Period																		

■ ■ ■ Projected Building Construction Schedule
■ ■ ■ Potential Public Realm Construction Period

Financing and Funding

A coordinated funding and financing strategy must be agreed upon by the Office of the Deputy Mayor for Economic Development, the Office of Planning, the Department of Transportation, NCRC, WMATA, and the private development principals. This strategy should include, and leverage:

- coordination of public realm improvements with new development,
- coordination and phasing with planned and future capital improvements,
- creation of local improvement district and use of tax increment financing for infrastructure and public realm improvements,
- WMATA transit area improvement funds,
- additional capital allocations from the District of Columbia,
- coordination of DC Commission of Arts & Humanities artscape improvements.

Public Realm Management

Generally, the regular management and maintenance of curbside improvements within the 'public realm' is the responsibility of abutting private property owners. In complement, the Public Realm Framework recommends the formation of a Business Improvement District to oversee the management and maintenance of public improvements within the 14th Street 'core area' and improvements funded under the proposed Local Improvement District. This organization should evolve from current community associations and established Columbia Heights institutional interests and business community organizations such as CHAMPS, the Columbia Heights Development Task Force and a proposed Main Street organization.

Responsibilities of this body must conform to established District policy and be tailored to the proposed physical improvements of this Framework. Responsibility for management and maintenance of the Civic Plaza should fall under the auspices of this organization while ownership of the Plaza shall be retained by the District of Columbia. The stewardship of this Plaza should extend to the creation and management of public-authorized programs for special uses and activities, including the Columbia Heights Farmers Market and a variety of cultural and entertainment programs which should be planned and staged throughout the year.

Acknowledgements

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COLUMBIA HEIGHTS

Public Realm Framework



Government of the District of Columbia
Office of Planning
October 2004

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**Public
Realm
Framework**

Prepared for



Government of the District of Columbia
Office of Planning

Prepared by

Zimmer Gunsul Frasca Partnership

October 2004

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Introduction

Public Realm

The domain of the urban environment that belongs to the public community; the civic spaces and the streets.

This framework establishes a unified approach to the public realm in Columbia Heights. It recommends improvements to public streets, spaces and amenities; opportunities to strengthen the identity of the neighborhood; and the necessary actions to integrate new development.

Project Goals and Work Scope

The Project Work Scope spanned a seven month period from late March thru October, 2003. It has been organized around the following sequence of Phases, which give the structure for this document:

Phase 1 - Neighborhood Structure and Character

Understand the Cultural and Physical Background and Evolving History of the Columbia Heights Neighborhood

- Conduct and Document Physical Assessment of Neighborhood
- Conduct Stakeholder Interviews
- Define Goals, Issues and Strategies
- Define Program for Public Realm Improvements

Phase 2 – Public Realm Framework

Create an Urban Design Framework Plan for the Public Realm which defines a "design identity" for the public places and streetscapes that capture the unique qualities of Columbia Heights, and which integrate public space with private and public development.

- Develop and Define Public Realm Studies
- Develop Guideline and Technical Requirements for Public Improvements

Phase 3 – Implementation

Develop and Recommend Implementation Strategies for the Plan that coordinate public and private investment.

- Develop Cost Estimates for Construction and Installation of Improvements
- Develop Phasing Plan and Coordination Strategy
- Develop Strategies for Public Realm Management and Maintenance

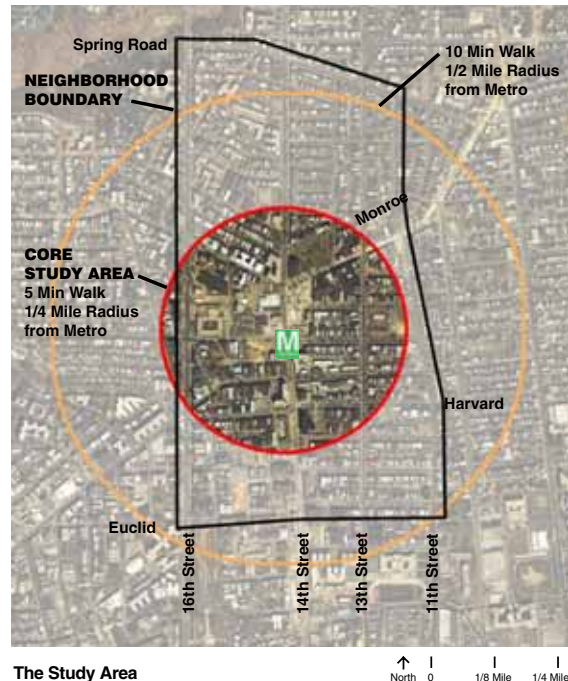
Planning Process

To create a plan which is embraced by the entire community, the Government of the District of Columbia actively engaged neighborhood residents and stakeholders in an open and integrated public discussion regarding community interests and needs.

The District Office of Planning (DC-OP) and the design consultants, Zimmer Gunsul Frasca (ZGF), with the assistance of Ward 1 ANC leaders, invited and formed an ad hoc Advisory Committee made up of fifteen residents and several ANC members. The committee provided generous guidance with respect to public outreach for the project and specifically with the conduct of three public workshops and a final public presentation. It further defined a Project Design Sub-Committee to work with DC-OP and ZGF, and met an additional five times to inform and advise the development of the Framework and the design of public space. Additionally, DC-OP and ZGF conducted many interviews with individual representatives and stakeholder entities, and held many meetings with private development interests for the RLA parcels. Project progress was reviewed in a series of meetings with DC-OP, District Department of Transportation (DDOT), National Capital Revitalization Corporation (NCRC), and the Office of the Deputy Mayor for Economic Development (ODM-ED).

The Public Realm Framework process was conducted in conjunction with the Mount Pleasant and Columbia Heights Transportation Plan prepared under the auspices of DDOT and their consultants, Cambridge Systematics. The work plans, planning and design strategies, public workshops and final design proposals were coordinated to provide a joint set of recommendations to the District for the future development of the public infrastructure in Columbia Heights.

The DC Commission on the Arts and Humanities issued a public offering to join the Design Team for the Framework Plan in the development of design proposals, with the objective that the process could lead to the award of one or more significant installations of Public Art in the Core Area of the Plan.



The Study Area



The Neighborhood's location within the District of Columbia

Columbia Heights is located in the heart of District Ward 1 and is surrounded by the neighborhoods of Mount Pleasant, Park View, Pleasant Plains, and Cardozo/Shaw. Its boundaries are Spring Road to the north, Euclid Avenue to the south, 16th Street to the west, and 11th Street to the east. The neighborhood's central corridor, 14th Street, is a traditional neighborhood commercial-retail street. The 'Core Study Area' is 1/4 mile radius, centered on the Metro stop at 14th and Irving Streets.

Consistent with the District's planning goals, the Framework has been developed according to the principles of Transit Oriented Development (T.O.D.). T.O.D. encourages a higher density and diversified complement of active street-level uses, in order to foster a high quality of cultural diversity and a greater vitality of public life.



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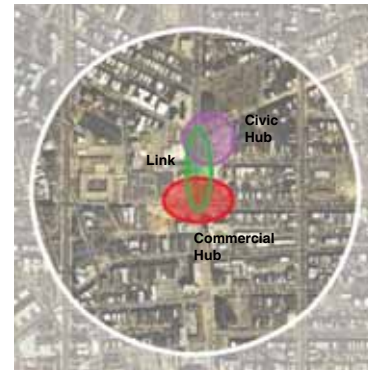
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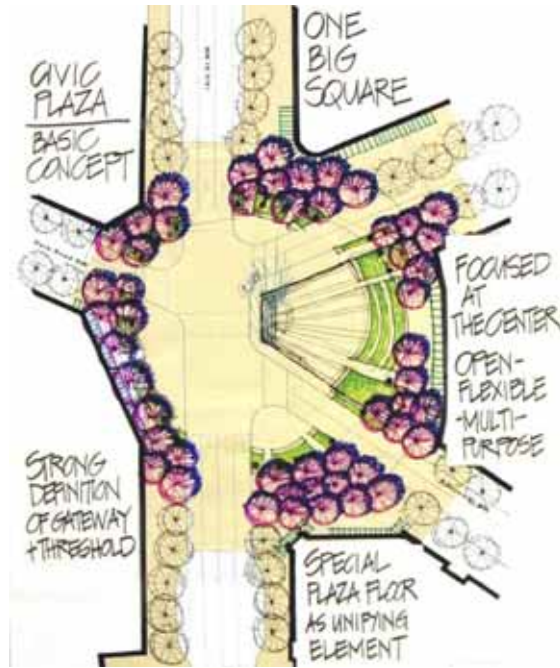
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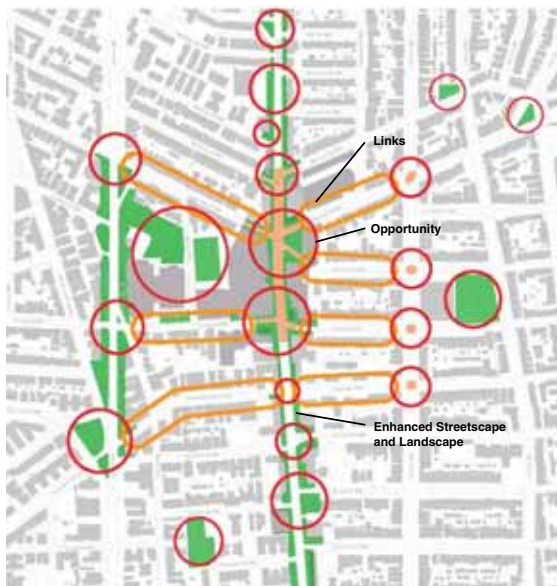
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The Civic Plaza Design Concept



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4. Temporal / Changing changing emerging	evolving revival	history/historic 'renaissance'
5. Humanitarian soulful we/the people as 'art'	personal people-focused (not object-focused)	intimate

The words used by residents to describe their neighborhood



The stepped lawn will provide an opportunity to watch neighborhood activity



A flowering, ornamental tree will ring the plaza



The interactive fountain will provide a dynamic place for children to play and relief from summertime heat



The sculptural 'flowers' will provide shading and ornamental lighting

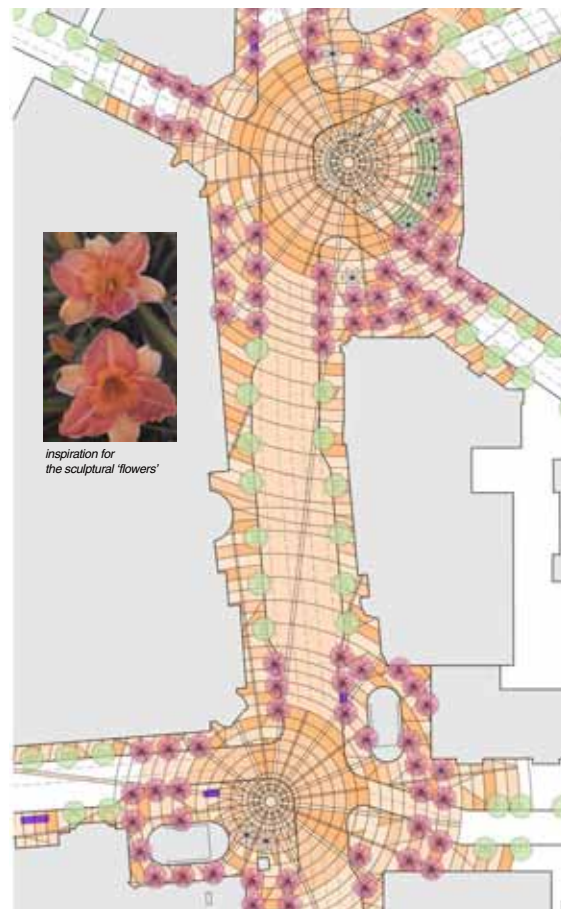


The Civic Plaza

The Design of the Civic Plaza and Metro Plazas

The Plaza is located in the triangular shaped parcel at the intersection of Kenyon St., Park Road, and 14th Street. It is approximately 12,000 square feet in area. The design theme is based upon a 'kaleidoscope' which symbolizes and embodies the rich diversity of the community.

- The Plaza is organized around a strong geometric paving pattern, derived from overlaying a kaleidoscopic image upon a radial-concentric design which emanates from the center.
- The paving extends across the intersection, unifying the entire open space and new buildings, to create the sense of a much larger and cohesive public space. Thus, the plaza can comprise the small triangular park; or include the sidewalks across Kenyon St. and Park Road; or become the entire space encompassed in the pattern. Occasionally, streets may be closed and the entire space may be used to host community events. The triangular plaza can support 800 - 1000 people, while the entire, expanded plaza space can support up to 3,000 people for special community events.
- The Plaza is predominantly 'hardscaped' and is designed to be flexible enough to accommodate a wide range of uses.
- A special circle, thirty feet in diameter is located at the center of the Plaza. Here, the Plaza forms a shallow bowl and is depressed approximately 12 inches and surrounded by seat walls on three sides. Located within this circle is the primary feature of the Plaza – a public, interactive fountain with informally placed jets of water that may be programmed in a syncopated rhythm. The fountain symbolizes the spontaneity and vitality of the Columbia Heights neighborhood. It is also designed to have several 'phases of intensity', which facilitate diverse uses of the Plaza.
- The 'hardscape' is softened by the use of a terraced lawn -- a series of grassy steps which also provide an opportunity for seating and relaxation. The steps are five feet wide, terrace upwards away from the plaza center in low steps, and are framed in low concrete walls which function as seat walls to the adjacent sidewalk cafes.
- The entire edge of the Plaza is defined by planted rings of flowering ornamental trees which surround the east side of the lawn, and extend across Park Road, Kenyon Street and 14th Street.
- The edge of the Plaza is further defined by a ring of large-scale sculptural 'flowers', which provide shade canopies and ornamental lighting within the area of the lawn, and serve as formal 'signature' elements. These flowers are designed in the form of day lilies and constructed of metal 'stems' with 'petals' constructed of a tensile fabric over light-weight metal frames. The ring also extends across Park Road and Kenyon Street, where single flowers are located to mark those portions of the extended Plaza.
- The radial-concentric pattern extends down 14th Street to connect with the Metro Station entry plazas. A radial element from the Civic Plaza connects to a smaller second 'center' of the pattern at the Metro entries. The 14th St. / Irving St. intersection is also ringed with ornamental flowering trees to mark the gateway into the 'core area' from Metro-rail. Additional 'flower' elements also are placed at the Metro plazas to strengthen the relationship with the Civic Plaza.

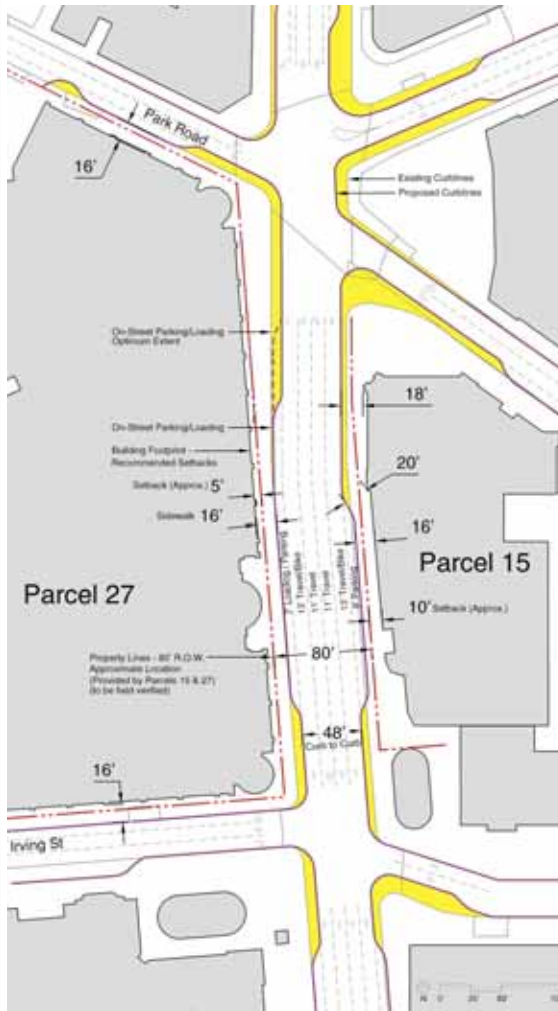


inspiration for the sculptural 'flowers'

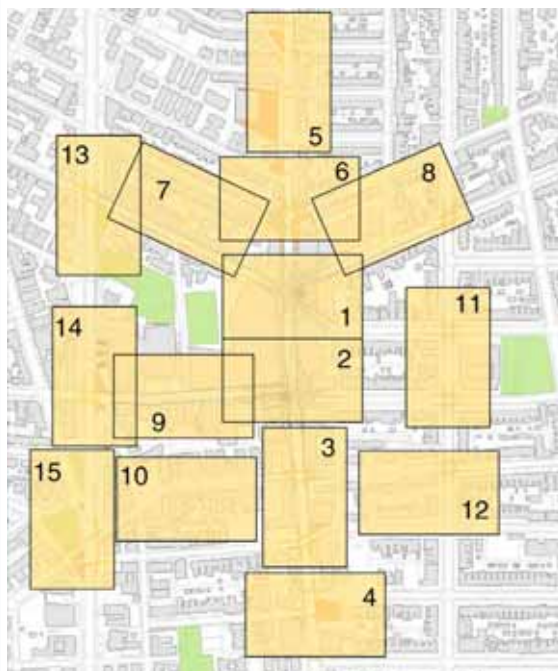
The unique paving pattern, together with the sculptural 'flowers', ornamental trees, and the integration of public art are intended to establish and reinforce a special civic place and community identity for the core of Columbia Heights.



The Metro Station Plazas



14th Street Right-of-Way: The areas in yellow identify curb extensions



Focus Area Design Plans



Traditional Lighting



Custom Bus Shelters

14th Street Right-of-Way Design

Within the 'core area', 14th street should be reconfigured to establish balanced access for all modes to and around the Metro Station entrances and to support traffic patterns which complement the neighborhood's commercial core. The reconfiguration of travel lanes will also provide a gradual transition from four travel lanes south of Irving Street to the existing two travel lanes north of the Monroe Street.

14th Street, between Irving Street and the Park/Kenyon intersection, should accommodate two 11 foot wide travel lanes in the center, two 14 foot wide combined travel-bike lanes at the outside, and on-street parking lanes along both sides of the street. This allows the connection of bike lanes to the Metro Plazas and on-street parking which reinforces the commercial uses along the street and enhances the pedestrian environment. Sidewalks must be a minimum 16 feet wide in the 'core area' to accommodate a relatively generous through-pedestrian zone and a furnishing zone along the curb. Door swings, cafe tables, and all utilities, should be held back from the 16 foot dimension. Where possible, a 20 foot wide sidewalk is recommended which would allow a four foot zone adjacent to the building to be used by the businesses to "enliven" their storefronts with tables or displays. Setbacks of adjacent properties may be required to provide the desired sidewalk widths.

Each of the intersections along 14th Street, from Monroe Street to Girard Street, should be reconfigured with generous curb extensions to enhance pedestrian safety and strengthen the public realm. The Transportation Study includes additional recommendations for improving pedestrian safety such as modifications to traffic signalization.

- The Monroe Street / 14th Street intersection should be reconfigured to provide better and safer access along Monroe across 14th Street.
- 14th Street from Monroe Street to Park Road should include two travel lanes, two dedicated bike lanes, and on-street parking lanes on both sides of the street. South-bound at Park Road, the parking lane converts to a dedicated right-turn lane.
- The Kenyon Street / Park Road / 14th Street intersection should be reconfigured to improve pedestrian safety. West-bound traffic at the Park Road / 14th Street intersection will permit a right-turn only northbound onto 14th Street. Kenyon Street will be reduced to two travel lanes at 14th Street.
- North-bound travel on 14th Street at the Park Road / Kenyon Street intersection will have a dedicated left-turn lane and one through travel lane which extends north to Monroe.
- The Irving Street / 14th Street intersection is also improved by narrowing the roadbed to provide a south-bound left turn lane and one through travel lane.

Focus Area Design Plans

The Focus Area Design Plans detail recommendations for specific areas throughout the neighborhood including special intersections and opportunity areas, street trees, lighting and sidewalks. The Plans are organized by priority. First priority areas include Plans 1 through 8. Second priority areas are Plans 9 through 12. Third priority areas are Plans 13 through 15.



High quality streetscapes and adjacent uses will enhance the pedestrian environment

Streetscape Design

The streetscape design for Columbia Heights is derived from the design of the core area. Streetscape elements include the three different scales of the standard DC ornamental street light poles placed at slightly more dense spacing in the core area to emphasize its importance.

Street trees along 14th Street will be London Plane or Linden trees, which have a slightly vertical form, and will provide a generous canopy for shading along the sidewalk through the commercial center of Columbia Heights. The plane trees will be placed at 40 foot intervals along 14th Street, within curbside planters filled with hearty ground-cover plantings and edged with DC standard iron hoops.

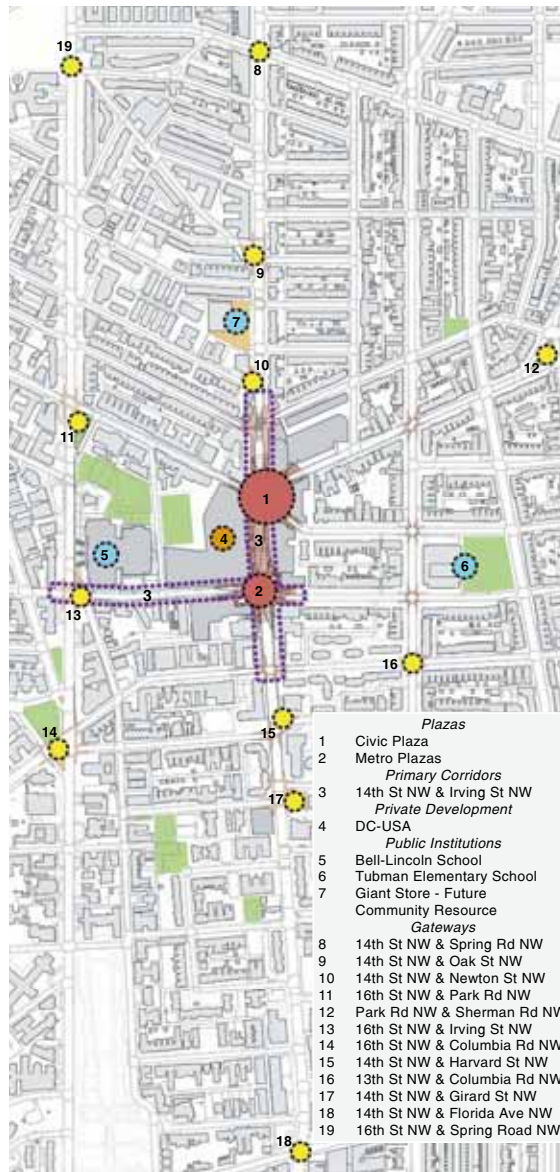
Within the core area, the sidewalks are proposed to be at least 16 feet in width to provide space for a generous pedestrian zone, and for a furnishing zone along the street edge for street trees, ornamental street lights, and occasional benches, planters and bus shelters.

Public Art Framework

The DC Commission on the Arts and Humanities supported the selection of two artists to work with the Design Team on integrating public art concepts into the Public Realm. A public art framework was developed to identify opportunities in the community for public art installations. In addition, several conceptual plans for public art were created. These include: enhancements to paving and seating, custom designed street furnishings for lighting, shading and community history.



Potential Public Art Mosaics



The Public Art Framework



Potential Art Screens - Community Portraits and Masks



Preliminary Construction Budget Estimate Core Area Site Plan

Preliminary Construction Budget Estimates for Public Realm Elements

The construction costs for public realm improvements within the 'core area' have been developed in consultation with the DC Office of Planning, the District Department of Transportation and the Department of Public Works. They are based upon relevant construction costs for similar public right-of-way projects.

Construction costs for public improvements within the 'core area' are estimated at approximately \$6.0 to \$6.4 million. Consistent with conventional practice for costs planned at this stage of project development, these estimates include a 20% design/construction contingency.

The construction costs itemize the public right-of-way costs in relation to each adjoining development parcel, and are based upon area calculations for the right-of-way. Total project costs should be budgeted upon existing standard DC DDOT and DPW practices. Total project costs are estimated at \$7.2 to \$8 million, 20-25% higher than base construction costs.

Public Realm Phasing Projected Construction Schedule

Phasing Diagram LD	RLA Parcels	Project Name	2003				2004				2005				2006			
			Su	Fa	Wi	Sp	Su	Fa	Wi	Sp	Su	Fa	Wi	Sp	Su	Fa	Wi	Sp
Curbside Improvements Development Projects																		
A	29	Tivoli Theater																
B	Metro	Triangle II																
C	38	Civic Plaza																
D	15	Donatelli & Klein																
	26	Donatelli & Klein																
E	27	DC/USA																
F	24	Dance Institute																
Curbside Improvements - Adjacent to Existing Development																		
A1		Riggs																
D1		CVS																
F1		NE 14th St																
Streetside Improvements - 14th Street NW																		
G1		14th / Monroe																
G2		14th / Park / Kenyon																
G3		14th Midblock																
G4		14th / Irving																
Improvements Adjacent to Core Area																		
H		Bell Lincoln School																
J		Irving St																
K		Park Road																
Projected Building Construction Schedule																		
Potential Public Realm Construction Period																		

■ ■ ■ Projected Building Construction Schedule
■ ■ ■ Potential Public Realm Construction Period

Financing and Funding

A coordinated funding and financing strategy must be agreed upon by the Office of the Deputy Mayor for Economic Development, the Office of Planning, the Department of Transportation, NCRC, WMATA, and the private development principals. This strategy should include, and leverage:

- coordination of public realm improvements with new development,
- coordination and phasing with planned and future capital improvements,
- creation of local improvement district and use of tax increment financing for infrastructure and public realm improvements,
- WMATA transit area improvement funds,
- additional capital allocations from the District of Columbia,
- coordination of DC Commission of Arts & Humanities artscape improvements.

Public Realm Management

Generally, the regular management and maintenance of curbside improvements within the 'public realm' is the responsibility of abutting private property owners. In complement, the Public Realm Framework recommends the formation of a Business Improvement District to oversee the management and maintenance of public improvements within the 14th Street 'core area' and improvements funded under the proposed Local Improvement District. This organization should evolve from current community associations and established Columbia Heights institutional interests and business community organizations such as CHAMPS, the Columbia Heights Development Task Force and a proposed Main Street organization.

Responsibilities of this body must conform to established District policy and be tailored to the proposed physical improvements of this Framework. Responsibility for management and maintenance of the Civic Plaza should fall under the auspices of this organization while ownership of the Plaza shall be retained by the District of Columbia. The stewardship of this Plaza should extend to the creation and management of public-authorized programs for special uses and activities, including the Columbia Heights Farmers Market and a variety of cultural and entertainment programs which should be planned and staged throughout the year.

1 Neighborhood Context



Neighborhood Structure
Street Character Framework
Transportation Assessment
Community Identity

The Columbia Heights Neighborhood

Columbia Heights is changing rapidly as it attracts new housing, significant retail development and an influx of new residents.

The primary commercial corridor of the neighborhood, 14th Street NW, was significantly damaged in the 1968 riots following the assassination of Martin Luther King Jr. Re-investment in the neighborhood core over the following thirty years has been minimal.

In 1997, residents and stakeholders in Columbia Heights participated in a community charrette to create a redevelopment strategy for the neighborhood. Since the charrette, the neighborhood has been the focus of a major neighborhood redevelopment effort by the government of the District of Columbia and other public development entities, including the National Capital Revitalization Corporation (NCRC), the RLA Revitalization Corporation (RLA) and the Washington Metropolitan Area Transit Authority.

In 1999, WMATA opened the Columbia Heights Metro Station. This has helped connect the area to the rest of the region through the Green Line rail service and has increased the livability of the neighborhood.

This chapter provides a summary assessment of the neighborhood and is divided into three sections: *Neighborhood Structure*, *Street Character Framework*, and *Community Identity*.

- The *Neighborhood Structure* section provides an overview of the study area.
- The *Street Character Framework* identifies the hierarchy of streets in the neighborhood and provides a summary of the Transportation Plan
- The *Community Identity* section summarizes the public workshops held to understand residents' concerns and desires for the public realm and redevelopment in the neighborhood.



Neighborhood Art



Redevelopment Site along 14th Street



Infill Development on Park Road



14th Street Commercial Corridor, 1940s

Source: 1997 Community-Based Plan, WAF



14th Street, NW, April 1968

Source: 1997 Community-Based Plan, WAF



West side of 14th Street NW, 2003



The Green Line Metro Station



Neighborhood Gateway at 16th Street

A Community Based Plan for the Columbia Heights Metro Station Area, 1997



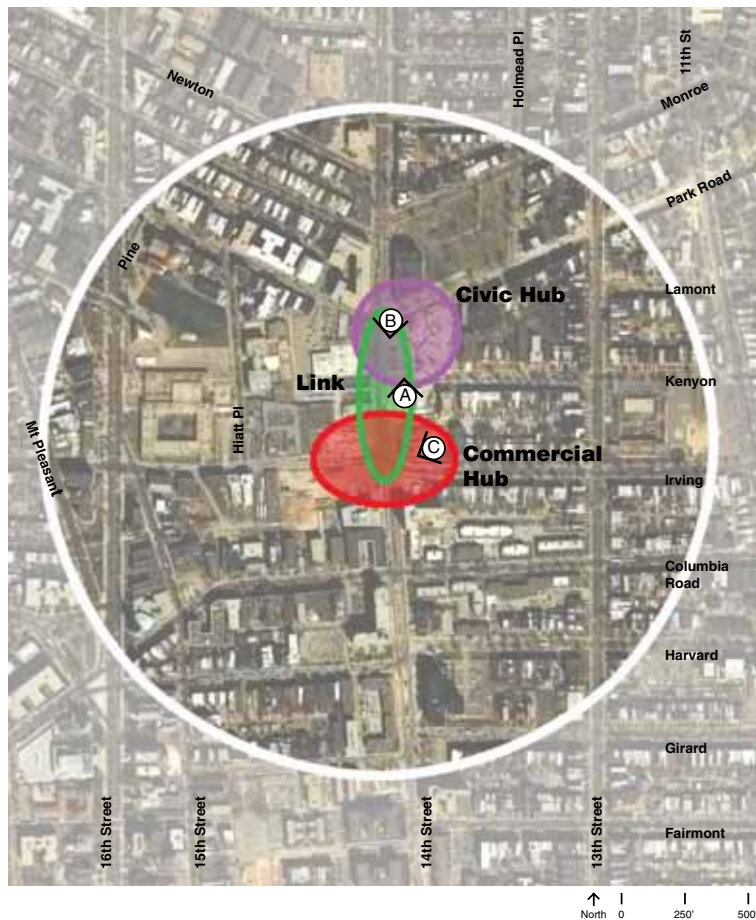
(A) The Civic Hub
Looking toward the Civic Hub Site from the
south along 14th St.



(B) The Link
14th St. looking south from the Civic Hub Site



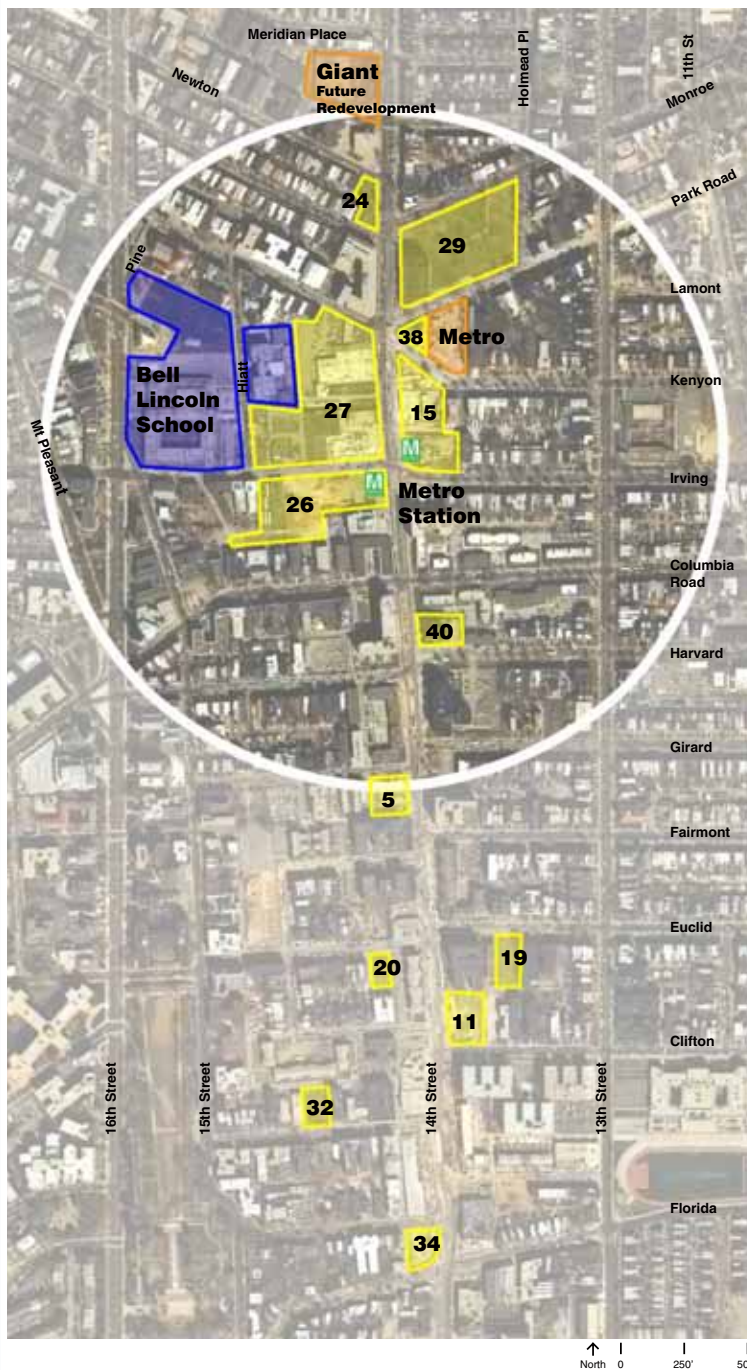
(C) The Commercial Hub
The west side of 14th St. and Irving St.
Intersection



The Public Realm Framework recommendations build on the Core Concept Diagram (above) and the principles identified in *A Community-Based Plan for the Columbia Heights Metro Station Area*, completed in 1997. The principles in bold italics are specifically addressed in the Framework's recommendations.

The Underlying Principles of the 1997 Plan

- Development should serve residents first, then tourists and other visitors.***
- Community-oriented retail stores and services should be given priority.
- Land use and location decisions should be designed to create a lively, well-shaded streetscape, both day and night.***
- Operating hours of shops, movie theaters and restaurants should be coordinated, and the location and hours of vendors managed to allow this to happen.
- Facades of new structures should be aligned at the sidewalk edge, not farther back.***
- Parking should be underground or hidden behind retail structures.***
- Ground level properties facing 14th Street should be retail, restaurants or other lively walk-in uses.***
- Large retail establishments should have a minimum street frontage. No blank facades allowed.***
- There should be some form of plaza or green space.***
- New structures should be architecturally compatible with older ones; their scales should blend. Existing zoning heights and densities should be maintained.***
- Vehicular traffic flow should be improved.***
- The Tivoli Theater should be restored and/or adapted for re-use.***
- The international ambiance of the neighborhood should be developed as a citywide draw.***
- Demolition of existing structures should be controlled.
- Rehabilitating existing housing should take precedence over new construction.
- A one-stop community services center should be developed along with a police substation and a new post office.
- Innovative ways of dealing with crime and personal security problems are needed.
- There should be two focal points along 14th Street; a civic & cultural heart in the Tivoli Area, and a commercial heart at the Metro Station.***
- Consideration should be given to upgrading and enhancing the existing educational institutions so they can meet the needs of the entire community.



Redevelopment Activity

(RLARC Development Parcels identified in yellow)

There are approximately fourteen acres in various stages of development within walking distance of the Columbia Heights Metro station. The projected development includes over 600 new housing units, approximately 650,000 square feet of retail space, 30,000 square feet of office space, approximately 2000 parking spaces and a new 800 student public middle and high school.

Parcel Number	Project
• Parcel 24	Dance Institute
• Parcel 29	Tivoli Partners
• Parcel 27	DC USA
• Parcel 38	Civic Plaza Site
• Metro	Triangle, II
• Parcels 15 & 26	Columbia Heights Ventures
• Parcel 40	Urban League
• Parcel 5	NPCDC
• Parcel 19 & 32	Triangle, II
• Parcel 11	Duron, Inc.
• Parcel 20 & 34	In Progress

Existing Zoning

The existing zoning and land uses in the neighborhood support the development of 14th Street as the commercial core surrounded by residential development.

- GOV Major Federal Tracts
- Streets
- PUDs
- Area of Overlay Districts
- Water
- Buildings

- | Zoning | Description |
|--------|--|
| C-1 | Neighborhood Shopping |
| C-2A | Community business center low residential density |
| C-2B | Community business center medium density |
| C-2C | Community business center high density |
| C-3A | Medium high major business and employment |
| C-3B | Medium high major business and employment |
| C-3C | High high major business and employment |
| C-4 | Central business district |
| C-5 | Public Transportation Business Development |
| C-6A | Low high commercial and light manufacturing |
| C-6B | Medium high commercial and light manufacturing |
| C-6C | High high commercial and light manufacturing |
| C-7 | Neighborhood commercial, retail, office & light industrial use |
| I-1 | General industry |
| R-1A | Single family detached dwellings |
| R-1B | Single family detached dwellings |
| R-2 | Single family detached dwellings |
| R-3 | Two dwellings & Det. |
| R-4 | Four dwellings & Det. |
| R-5A | Low density apartments |
| R-5B | Medium density apartment houses |
| R-5C | Medium density apartment houses |
| R-6A | High density |
| R-6B | Medium density residential limited office |
| R-6C | Medium density residential limited office |
| R-7 | Low density mixed residential-commercial |
| R-8 | High density mixed residential-commercial |

Prepared By The District of Columbia Office of Zoning



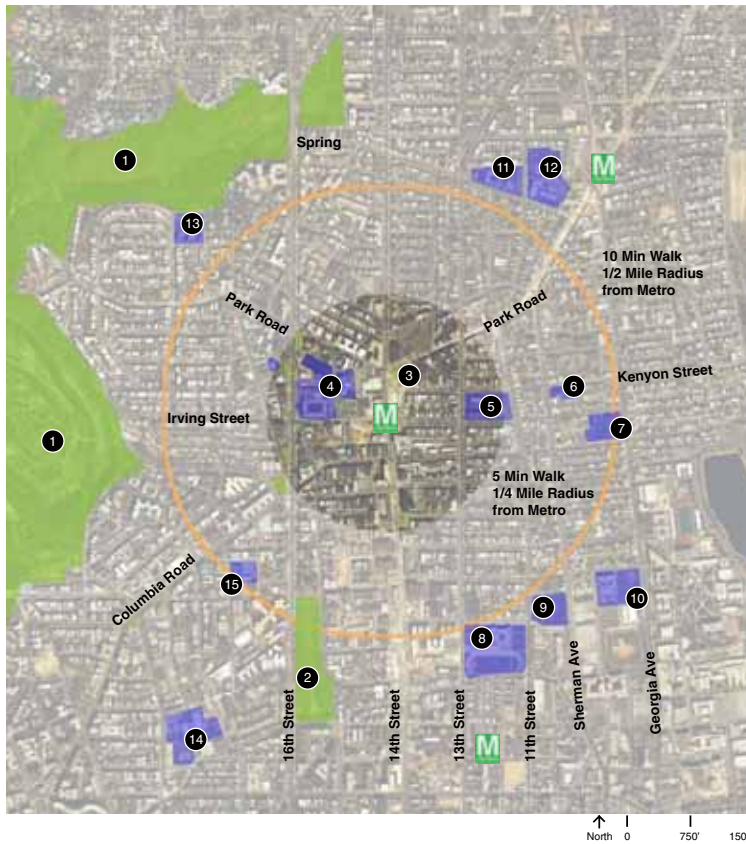
Existing and Proposed Land Uses

- Residential
- Mixed-Use (P=Proposed)
- Commercial
- Institutional



Neighborhood Public Open Space, Schools and Libraries

The neighborhood includes numerous schools within close proximity to the Metro station and a wealth of civic institutions that reflect the diversity of the community.



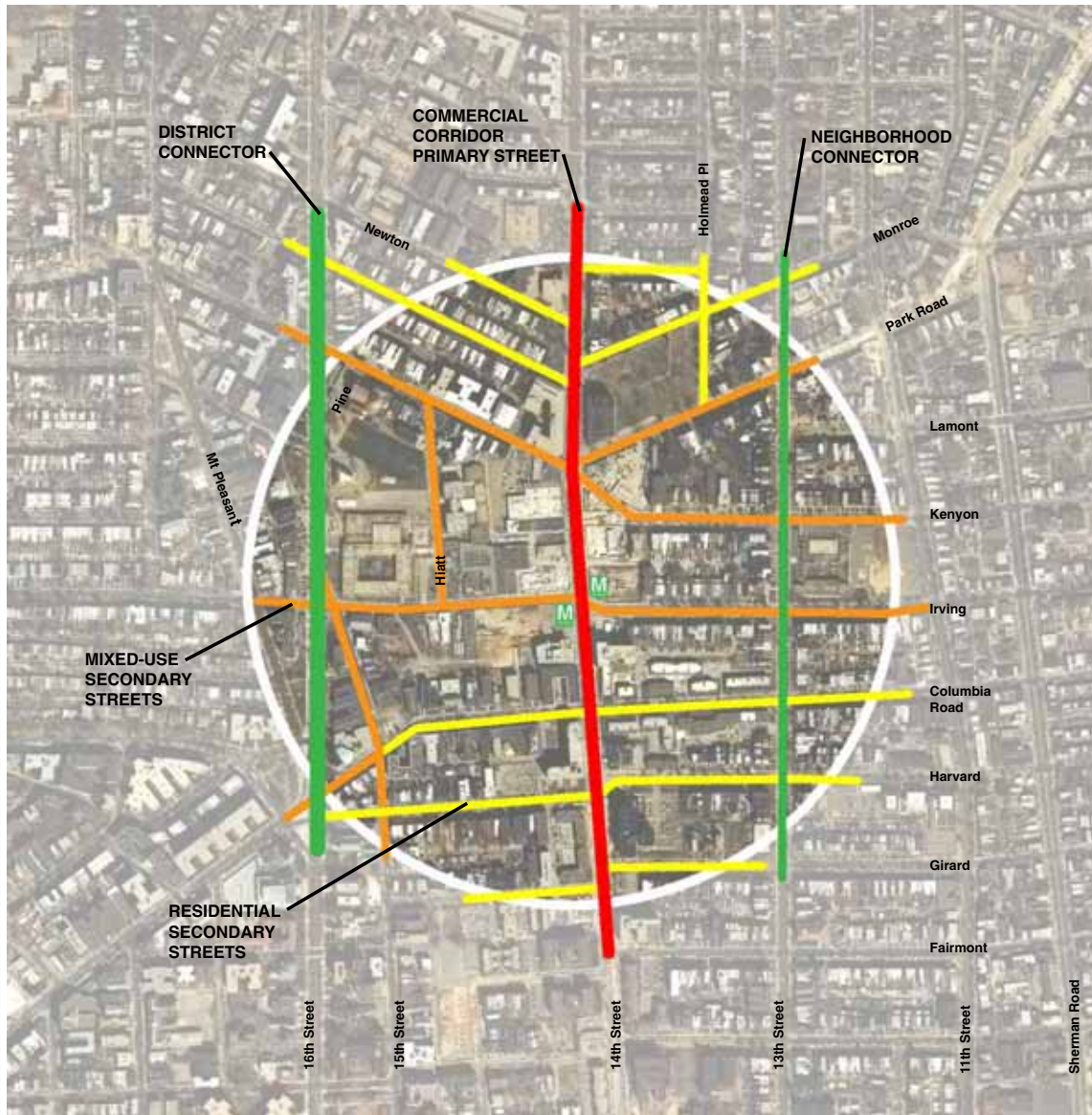
- 1 Rock Creek Park
- 2 Meridian Hill Park
- 3 Future Civic Plaza
- 4 Bell Multicultural SHS / Lincoln Middle School
- 5 Tubman Elementary School
- 6 Bruce School
- 7 Bruce-Monroe Elementary School
- 8 Cardozo Senior High School
- 9 Meyer Elementary School
- 10 Banneker Senior High School
- 11 Paul Robeson Special Education School
- 12 Raymond Elementary School
- 13 Bancroft Elementary School
- 14 Marie Reed Learning Center
- 15 H.D. Cooke Elementary School
- 16 Mt. Pleasant Library

Community Assets

- 1 National Baptist Church
- 2 Calvary - Casa del Pueblo United Methodist Church, Rorschach Theater
Central American Resource Center
- 3 The Next Step Public Charter School
Latin American Youth Center
- 4 Calvary Multicultural Center
- 5 All Souls Unitarian Church
- 6 La Clinica del Pueblo
- 7 Second Genesis - Help for Addiction
- 8 Capitol City Public Charter School (above CVS)
- 9 Church of Christ
- 10 Harriet Tubman Elementary School
- 11 Easter Seal Society - Child Development Center
- 12 Village II Early Childhood Development Center
- 13 True Family Education Center of D.C.
- 14 DC Department of Parks & Recreation Headquarters
- 15 Mount Pleasant Public Library
- 16 Meridian Hill Baptist Church
- 17 Neighbor's Consejo Community Center
- 18 Barbara Chambers Children's Center
- 19 La Casa
- 20 Bell Multicultural High School
- 21 Shrine of the Sacred Heart Catholic Church
- 22 Elsie Whitlow Stokes Community Freedom Public Charter School
- 23 Sacred Heart School
- 24 Canaan Baptist Church
- 25 Spanish Catholic Center
- 26 The Family Place
- 27 Catholic Charities McKenna House
- 28 Kelsey Temple Church of God in Christ
- 29 Mt Rona Baptist Church
- 30 Lincoln School
- 31 DC Fire Department
- 32 Development Corporation of Columbia Heights, DCCH
- 33 The Greater Washington Urban League (future site)
- 34 Tivoli Theater
- 35 St Stephens



Street Character Framework



Different types and intensities of land use have different urban design requirements. The Street Character Framework illustrates the classification and hierarchy of the public street system in the neighborhood and ensures that streets with different use and transportation functions are appropriately linked and guides the development of pedestrian amenities.

The Appendix also includes an assessment of the physical characteristics of each street within 1/4 mile of the Metro station in order to provide a basis for guiding future improvements. Summary findings from the concurrent Transportation Plan for each street are also included.

Transportation Plan Summary Assessment of Existing Conditions

Source: Cambridge Systematics



**Congestion
& Level of Service
- A.M. Peak**



**Congestion
& Level of Service
- P.M. Peak**



**Congestion
& Level of Service
- Weekend Mid-Day**

○ Intersection
— A, B, & C
— D
— E
— F



**Automobile Crashes
1999 - 2001**

• 1 - 4
• 5 - 14
• 15 - 24
• 25 - 34
• 35 - 45



**Pedestrian Counts
24 Hour**

• 401 - 1,500
• 1,501 - 2,250
• 2,251 - 3,000
• 3,001 - 3,500



**Pedestrian Crashes
and Potential Safety
Issue Areas
1997 - 2001**

• 1 - 2
• 3 - 4
• 5 - 6
• 7 - 10
• 11
Area of Concern

The diagrams above summarize the Transportation Plan's assessment of existing conditions. Key findings which have informed the development of the Public Realm Framework include:

- Intersections along 16th Street and the north end of 14th Street in the study area have the most congestion and lowest level of service. 14th Street in the core area currently provides an acceptable level of service
- The intersections with the highest number of automobile crashes are: 16th Street / Park Road, 16th Street / Irving Street, 14th Street / Park Road / Kenyon Street, and 13th Street / Columbia Road
- The intersection of 14th Street / Park Road / Kenyon has the highest pedestrian volume in the neighborhood followed by 14th Street / Irving St, 16th Street / Irving Street, and 14th Street / Columbia Road
- The highest number of pedestrian crashes occurs at the intersection of 16th Street / Irving Street. There are two potential safety issue areas: 16th Street between Harvard Street and Park Road, and 14th Street between Irving Street and Park Road.

Community Identity

A public realm which strengthens the identity of the Columbia Heights neighborhood is essential. To build upon the qualities of the existing neighborhood, a series of meetings with the Advisory Committee and a public workshop were held to provide opportunities for resident input.

The public workshop had two purposes. Maps and icon cards were used to hear from residents about their concerns and desires for physical improvements to the public realm: in particular, the 14th Street Corridor, the Civic Plaza and the adjacent streets. The workshop was also used to capture residents' impressions of the qualities and characteristics of Columbia Heights to help serve as a foundation for future improvements to the public realm.

Icon Cards - An Initial Menu



These icon cards, in addition to blank cards, were used by the community to identify concerns and opportunities for development of the public realm. Community members place cards on large maps to identify where these concerns and opportunities exist.

The Public Workshop



Neighborhood Issues

Summary:

- The north-south streets - 16th, 14th and 13th streets - need improved intersections and traffic calming.
- Street furnishings, such as trash cans and bus shelters are needed at select locations along the north-south streets.
- On-street parking on the Park Road and Columbia causes problems with through traffic.
- East-west streets with high pedestrian volumes need to have trash cans and better lighting for personal safety.



Neighborhood Gateways

Summary:

- Neighborhood residents identified western gateways along 16th Street, north and south gateways along 14th Street and eastern gateways at 13th Street and Sherman Road.
- The Green Line Metro entrance is also identified as a gateway.





Potential Enhancements along 14th Street NW

Summary:

- Several intersections along 14th were identified as potential locations for community enhancements, including: the triangular park at Oak Street, the existing Giant parking lot and the park at Girard.
- The intersections at Irving St, Columbia Road and Park Road should be identifiable as special intersections in the neighborhood.
- The Metro plazas should be enhanced with better bus facilities, public art and adjacent development which supports transit ridership.



Potential Elements in the Civic Plaza

Summary:

- Residents suggested that the civic plaza should be designed to accommodate opportunities for people watching, musical performances and special events, such as the farmers market. The design of the plaza should also reflect the diversity of the neighborhood with public art, unique signage and adjacent uses which enrich the plaza.

Note: The numbers in red identify the frequency selected for each element by the residents.

Neighborhood Recreation Opportunities

Summary:

- Residents suggested that enhancements to existing parks and school grounds should provide recreational opportunities in the neighborhood.



Desired Landscape Enhancements

Summary:

- Residents identified the desire for enhanced landscaping at the existing parks in the neighborhoods.
- Residents also identified the desire for additional street trees along the main east-west pedestrian streets in the neighborhood.



Defining Community Identity

The primary strategy of this plan has been to capture the essential and evolving character of Columbia Heights and to identify a 'design character' for the public realm.

The community identified the following neighborhood characteristics:

- a commitment to embrace the extraordinary cultural and economic diversity
- a sense of enthusiasm and vitality for the public realm
- a spirit of openness and tolerance
- an interest in promoting development which respects its past while looking forward to the future.

The residents also identified the following attributes:

- Multi-ethnic / multi-cultural district
- A neighborhood with historic qualities
- Quality residential fabric
- Commercial crossroads
- 'Sub-district' cultural/arts center
- Place of evolving / dynamic cultural / physical change
- Active neighborhood public involvement
- Public education cores: Bell, Lincoln, Cardozo
- Mass-transit-station hub / transit-oriented development
- Hilltop prominence / views to monumental core.

Community Identity

The community was also asked for words which describe the atmosphere of Columbia Heights. Their words have been organized into the categories shown below.

1. Cultural

multi-cultural
humanity
multi-generational
'kaleido-cultural'

multi-economic
community oriented
mix of faces
neighborhood

melting-pot
multi-ethnic
familiar faces
multi-lingual

2. Ambience / Atmospheric

Viva!
sense of conflict
urban
uncut/raw
chaos

'a lot going on'
unrefined
crowds
vitality
'spike lee joint'

crossroads
vibrancy
edgy-tension
density
skyline

3. Social

open
colors
accommodating
flexible
contrasts
multi-generational

tolerance
multi-purpose/use
old/new
accepting
non-linear

'kaleidoscope'
welcoming
shapes
diverse
rooftops

4. Temporal / Changing

changing
emerging

evolving
revival

history/ic
'renaissance'

5. Humanitarian

soulful
we/the people as 'art'

personal
people focused (not object)

intimate



2 Public Realm Framework



- Goals
- Concept
- Civic Plaza Design
- Focus Area Plans
- Street Character Framework
- Streetscape Design
- Public Art

Introduction

The Public Realm Framework is based on the principals of *A Community-Based Plan for the Columbia Heights Metro Station Area* completed in 1997.

The Framework has been developed through an intensive engagement process with community residents organizations, agency and the design team. Out of a set of clearly defined goals and objectives, a concept was developed to guide the design of the 'public realm'. Particular attention was given to the civic plaza, which is viewed as the center of neighborhood 'public realm' improvements. The concept translates key design opportunities into focused design area plans for the community.

Goals

- 1 **Strengthen Community Identity**
- 2 **Celebrate Diversity**
- 3 **Create a Lively Experience**
- 4 **Make it Easy to Walk**
- 5 **Make Transit Convenient**
- 6 **Make it Safe**

Objectives

1. **Strengthen Community Identity**
 - Establish a **clear definition** of the public realm.
 - Create a **civic/cultural heart** at a new public plaza and a **commercial heart** at the new Metro Station entrances.
 - Place priority on development of **community-oriented commercial uses**.
 - The **architectural character and scale of new development** should be compatible with existing character and density.
 - Develop high quality private improvements, with **good life-cycle cost characteristics**.
2. **Celebrate Diversity**
 - Create/perpetuate a special **design identity** for Columbia Heights - based upon its multi-cultural/multi-ethnic quality.
 - Enhance existing **cultural and educational uses**.
3. **Create a Lively Experience**
 - Encourage **retail uses at the ground level** in the civic/commercial heart, with generous transparency of facades.
 - **Commercial building facades** should align with the public R.O.W., while allowing generous sidewalks in the civic and commercial heart.
4. **Make it Easy to Walk**
 - Balance **access and safety requirements** for all transportation modes.
 - Create a clear **hierarchy of public streets**.
 - Establish **pedestrian priority** within the civic/cultural and commercial heart of Columbia Heights.
 - Create **nodes and linkages of public activity** along 14th Street, with generous sidewalks and high quality streetscape improvements.
5. **Make Transit Convenient**
 - Improve access to **mass transportation** throughout the neighborhood.
 - Establish high quality **transit-oriented development**.
 - Encourage **mixed-use development**, especially along the 14th Street corridor.
6. **Make the Community Safe**
 - Balance **access and safety requirements** for all transportation modes.
 - Improve **vehicular circulation and access** throughout the neighborhood.
 - Optimize **on-street parking** on all public streets.
 - Encourage development of **off-street parking**, located below grade or behind retail frontages. Optimize time-share parking concepts.

Public Realm Framework: Concept

The Public Realm Framework Concept illustrates the urban design strategy for Columbia Heights. It focuses on the 'core area' of the community – the Civic/Cultural and Commercial Heart" of the revitalized Columbia Heights – extending along the central 14th Street commercial corridor, from Columbia Road to Monroe Street. It centers on the creation of a new Civic Plaza, at the intersection of Park Road, Kenyon Street and 14th Street, immediately south of the renovated Tivoli Theatre. The design strategies in the Framework connect the Civic Plaza with Metro Station plazas and the adjacent development.

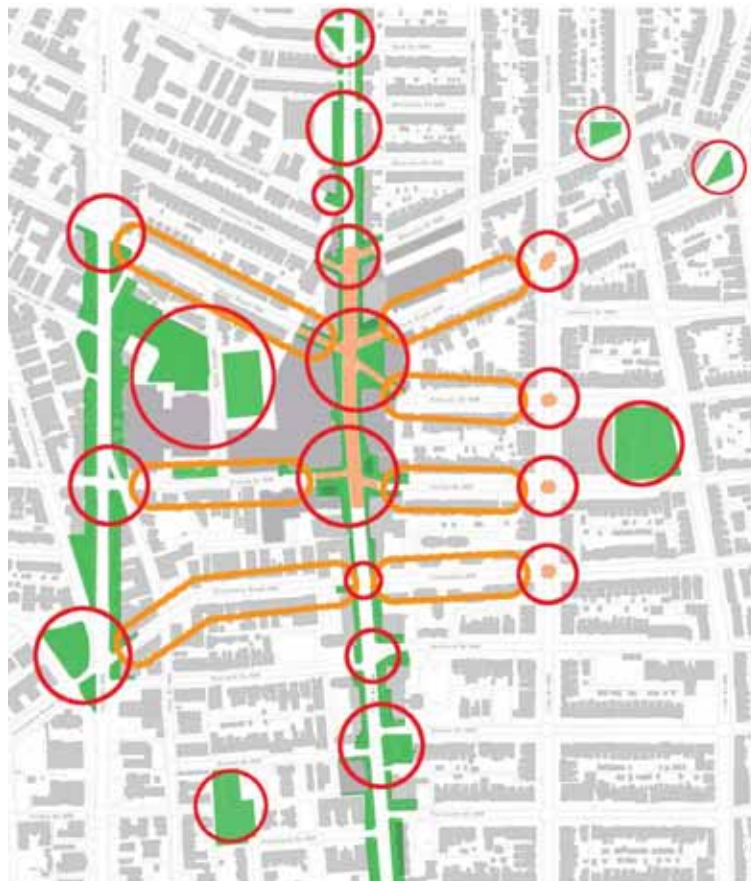
The design strategies were created to capture the community identity of Columbia Heights. The design of the 'core area' establishes a palette which can be replicated in symbolic ways at several key intersections in the neighborhood. These form gateways into the community which are connected through their design to the 'core area'.

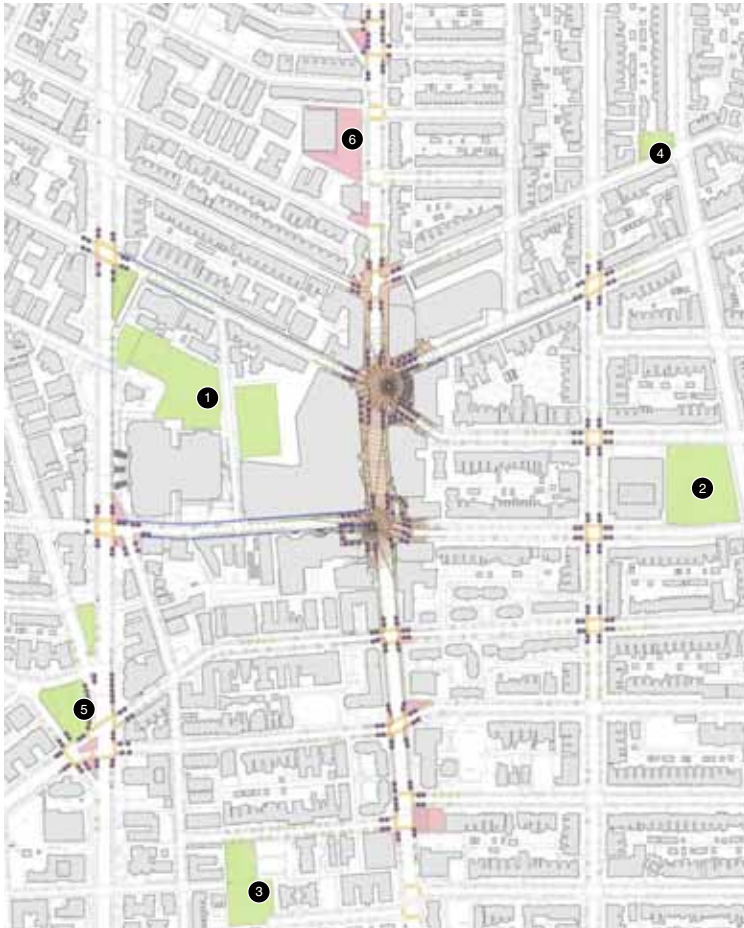


Key Design Opportunities

Key design opportunities are identified throughout the community. These locations offer opportunities for physical improvements which will unify the public realm. They include a sequence of intersections along 14th Street; key gateway entrances along 16th Street and 13th Street; and public and institutional open space, adjacent to schools and smaller scaled park spaces in Columbia Heights.

-  New Development
-  Opportunity Sites
-  Key Links
-  Enhanced Intersections
-  Enhanced Streetscape / Landscape



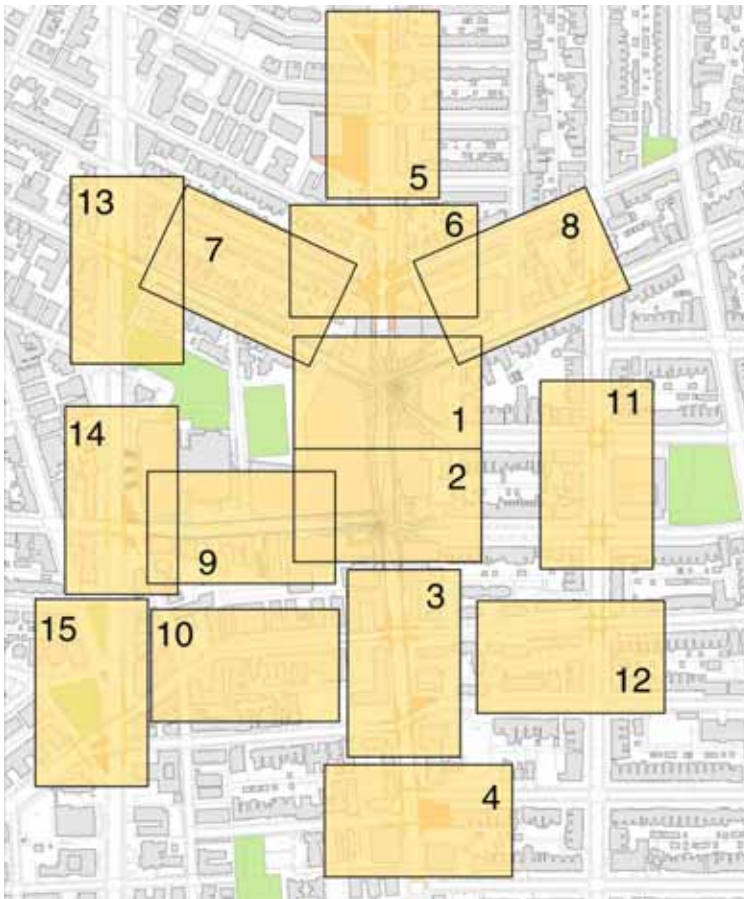


Public Open Space

The improvement of linkages to several public school sites, recreational spaces and public open spaces in Columbia Heights is explored in the framework.

Enhancement of these spaces, although not fully addressed in this framework, represent additional opportunities for revitalization and development necessary to enrich the community's entire public domain.

- 1 Bell Multicultural SHS / Lincoln Middle School
- 2 Tubman Elementary School
- 3 Girard Street Playground
- 4 Monroe Street Playground
- 5 16th Street and Columbia Road Park
- 6 Giant Site - Future Community Asset



Focus Area Design Plans

Design plans were developed for each of the key design opportunities. There are organized into 15 focus areas identified on the plan to the left. Each design plan complements the 'core area' of the Plan and illustrates prototypical public street right-of-way designs for the range of street types classified in the Neighborhood Assessment section of this document.

The design plans are presented in a sequence starting in the 'core area' along 14th Street, and extending through 16th Street, to the 'Secondary Mixed-Use Streets', 13th Street, and to the different scales of 'Residential Streets'.

Focus Areas #1 & #2

Special Design Opportunities

1a Civic Plaza

2a Metro Station Plazas

- Design descriptions - see text.

R.O.W. Improvements

C1 14th Street NW

- Refer to Street Prototypes for street trees, paving, lighting and furnishing information.

Design Concept

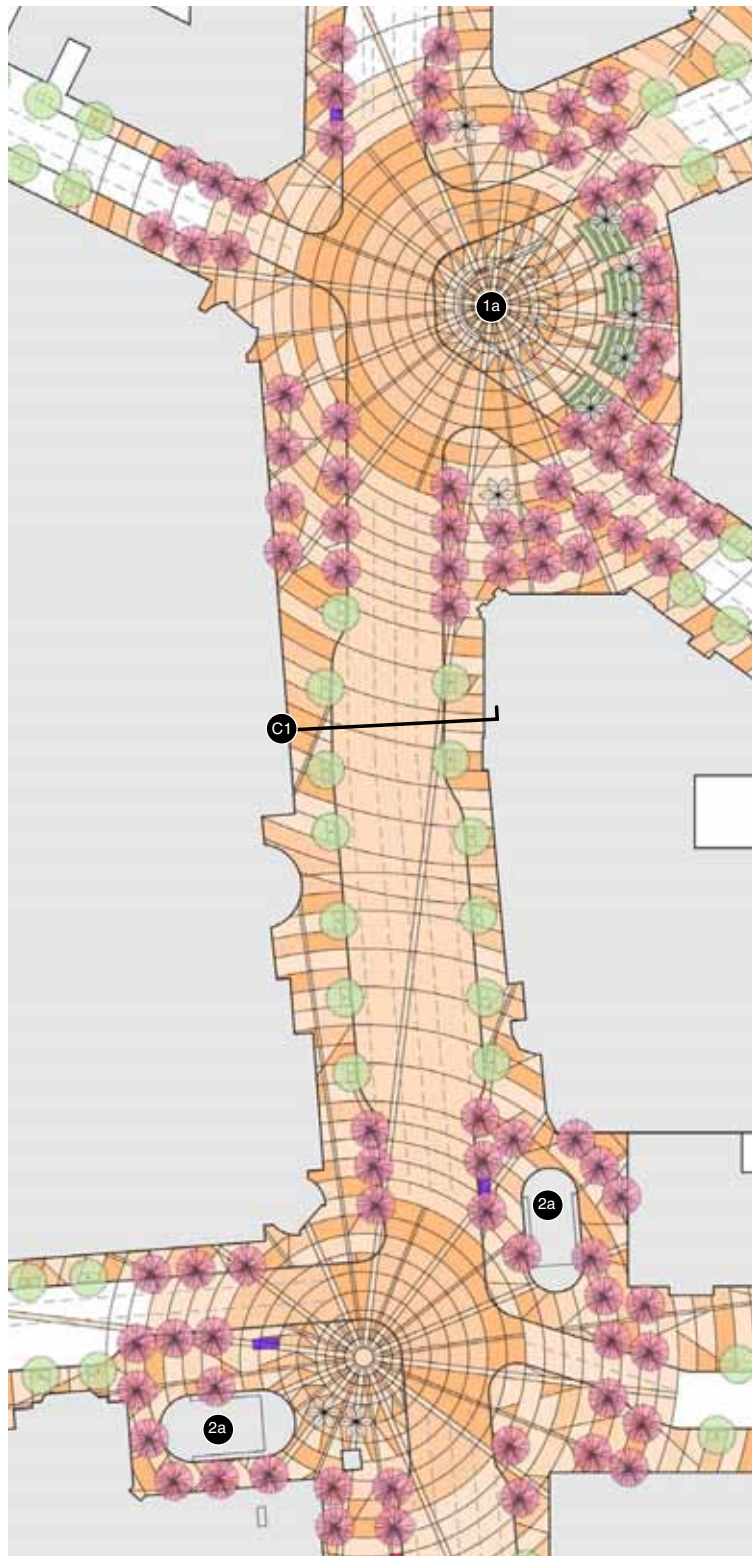
An initial list of key design ideas:

Civic Plaza

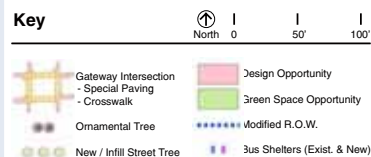
- One big 'square'
- Strong definition of gateway threshold
- Unique paving scheme
- Progression along 14th Street to 'square'
- Special landscape elements at perimeter of square
- Special architectural elements
- Special element feature at center
 - space can be occupied, open to all sides
 - operates as focus in various modes of street operation
 - operate as frame and focus of triangle space
- Majority in hardscape
- Special areas of softscape along edges and at select places within plaza
- Ground plane modulated to reinforce orientation to center
- Unique streetscape furnishings
- Unique trees
- Ground plane modulation to include seat walls
- Lighting to address various scales / uses and arranged to reinforce basic plan concept
- Potential to accommodate mix of activities at focus center
- Special, moveable chairs

Metro Plazas

- Functions more as transition space rather than civic gathering space
- Increased landscape to soften entrances



The Civic and Metro Plazas



The Design of the Civic Plaza and Metro Plazas

The Plaza is located in the triangular shaped parcel at the intersection of Kenyon St., Park Road, and 14th Street. It is approximately 12,000 square feet in area. The design theme is based upon a 'kaleidoscope' which symbolizes and embodies the rich diversity of the community.

- The Plaza is organized around a strong geometric paving pattern, derived from overlaying a kaleidoscopic image upon a radial-concentric design which emanates from the center.
- The paving extends across the intersection, unifying the entire open space and new buildings, to create the sense of a much larger and cohesive public space. Thus, the plaza can comprise the small triangular park; or include the sidewalks across Kenyon St. and Park Road; or become the entire space encompassed in the pattern. Occasionally, streets may be closed and the entire space may be used to host community events. The triangular plaza can support 800 - 1000 people, while the entire, expanded plaza space can support up to 3,000 people for special community events.
- The Plaza is predominantly 'hardscaped' and is designed to be flexible enough to accommodate a wide range of uses.
- A special circle, thirty feet in diameter is located at the center of the Plaza. Here, the Plaza forms a shallow bowl and is depressed approximately 12 inches and surrounded by seat walls on three sides. Located within this circle is the primary feature of the Plaza – a public, interactive fountain with informally placed jets of water that may be programmed in a syncopated rhythm. The fountain symbolizes the spontaneity and vitality of the Columbia Heights neighborhood. It is also designed to have several 'phases of intensity', which facilitate diverse uses of the Plaza.
- The 'hardscape' is softened by the use of a terraced lawn -- a series of grassy steps which also provide an opportunity for seating and relaxation. The steps are five feet wide, terrace upwards away from the plaza center in low steps, and are framed in low concrete walls. The back wall of the last step is 20 inches high, creating a low seat wall adjacent to 20'-25' wide promenade. This promenade fronts the Triangle II project, and the seat wall helps make it very suitable for outdoor cafe spaces.
- The entire edge of the Plaza is defined by planted rings of flowering ornamental trees which surround the east side of the lawn, and extend across Park Road, Kenyon Street and 14th Street. The trees are planted at 25' intervals.
- The edge of the Plaza is further defined by a ring of large-scale sculptural 'flowers', which provide shade canopies and ornamental lighting within the area of the lawn, and serve as formal 'signature' elements. The flowers are designed in the form of day lilies and constructed of metal 'stems' with 'petals' constructed of a tensile fabric over light-weight metal frames. The flowers are approximately 25 feet high, with a canopy width of approximately 20 feet. Five of the flowers form a ring around the lawn. The ring also extends across Park Road and Kenyon Street, where single flowers are located to mark those portions of the extended Plaza.
- The radial-concentric pattern extends down 14th Street to connect with the Metro Station entry plazas. A radial element from the Civic Plaza connects to a smaller second 'center' of the pattern at the Metro entries. The 14th St. / Irving St. intersection is also ringed with ornamental flowering trees to mark the gateway into the 'core area' from Metro-rail. Additional 'flower' elements also are placed at the Metro plazas to strengthen the relationship with the Civic Plaza.

The unique paving pattern, together with the 'flowers' and ornamental trees, are intended to establish and reinforce a special civic place and community identity for Columbia Heights.

The Civic Plaza, 'core area' and Metro Plazas have been designed to accommodate public art installations. A plan for public art is included at the end of this section. It highlights several concepts and proposals that can be integrated into the public realm.



The Civic Plaza

Aerial view from the northwest



The Metro Station Plazas

Aerial view from the southeast



The stepped lawn will provide an opportunity to watch neighborhood activity



A flowering, ornamental tree will be used to ring the Civic Plaza



The interactive fountain will provide a dynamic place for children to play as well as relief from summertime heat



The sculptural 'flowers' will provide shade canopies and ornamental lighting for the plaza

Potential Layout of Farmers' Market (stalls are shown in yellow)

The streetscape design for Columbia Heights is derived from the qualities of the 'core area'. Streetscape elements include the three different scales of the standard DC ornamental street light poles, to be extended typically throughout the neighborhood, but placed at slightly more dense spacing in the 'core area' to emphasize its importance. The typical spacing throughout the District and along 14th Street is at 60 foot intervals; the spacing within the core area, from Irving to Monroe Streets is proposed to be 40 foot.

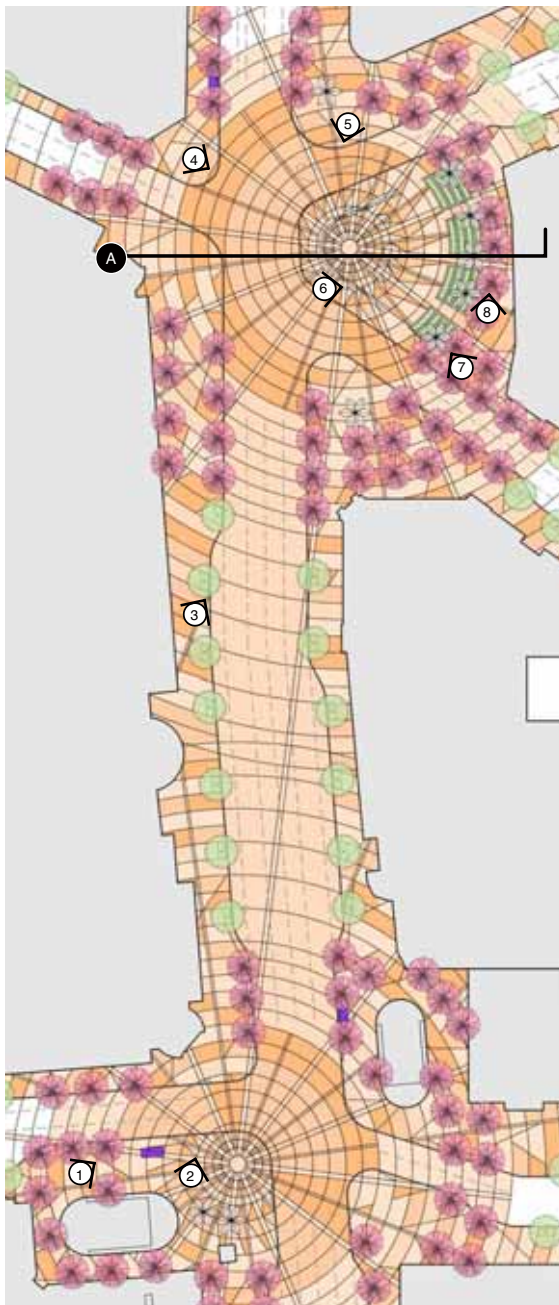
Within the 'core area', the sidewalks are proposed to be at least 16 feet in width, in order to provide space for a generous pedestrian zone, and for a planting zone along the street edge for street trees, ornamental street lights, and occasional benches, planters and bus shelters. Bench seating is selected from the DC standards. Bus Shelters are proposed to be custom-designed in the Core, but DC standard outside the Core.



ZIMMER-GUNSUL-FRASCA PARTNERSHIP



Section A - through the Plaza



Key Plan



1



2



3



4



5



6



7



8

Views of 14th Street and the Plazas

14th Street Right-of-Way Design

Within the 'core area', 14th street should be reconfigured to establish balanced access for all modes to and around the Metro Station entrances and to support traffic patterns which complement the neighborhood's commercial core. The reconfiguration of travel lanes will also provide a gradual transition from four travel lanes south of Irving Street to the existing two travel lanes north of the Monroe Street.

14th Street, between Irving Street and the Park/Kenyon intersection, should accommodate two 11 foot wide travel lanes in the center, two 14 foot wide combined travel-bike lanes at the outside, and on-street parking lanes along both sides of the street. This allows the connection of bike lanes to the Metro Plazas and on-street parking which reinforces the commercial uses along the street and enhances the pedestrian environment. Sidewalks must be a minimum 16 feet wide in the 'core area' to accommodate a relatively generous through-pedestrian zone and a furnishing zone along the curb. Door swings, cafe tables, and all utilities, should be held back from the 16 foot dimension. Where possible, a 20 foot wide sidewalk is recommended which would allow a four foot zone adjacent to the building to be used by the businesses to "enliven" their storefronts with tables or displays. Setbacks of adjacent properties may be required to provide the desired sidewalk widths. Appendix B illustrates the recommended setbacks for Parcel 27, the property west of 14th Street NW between Park Road NW and Irving Street NW.

Each of the intersections along 14th Street, from Monroe Street to Girard Street, should be reconfigured with generous curb extensions to enhance pedestrian safety and strengthen the public realm. The Transportation Study includes additional recommendations for improving pedestrian safety such as modifications to traffic signalization.

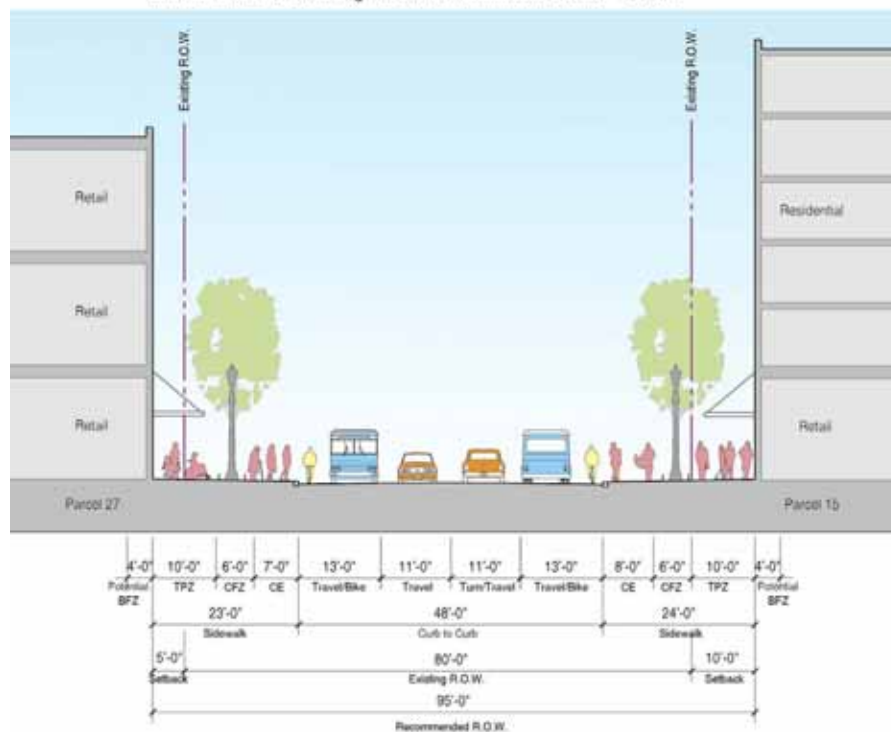
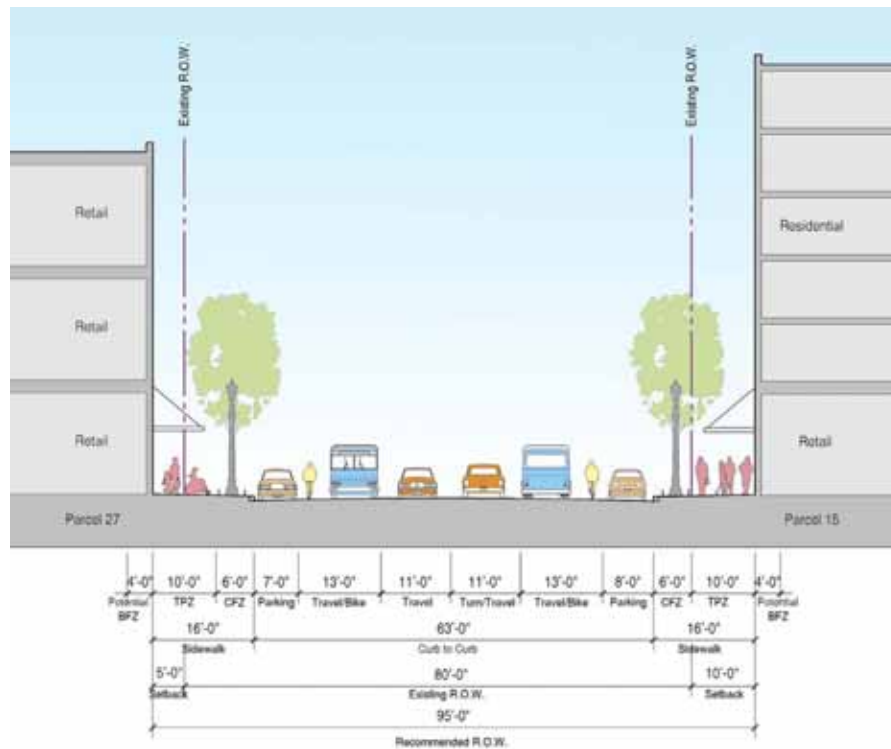
- The Monroe Street / 14th Street intersection should be reconfigured to provide better and safer access along Monroe across 14th Street.
- 14th Street from Monroe Street to Park Road should include two travel lanes, two dedicated bike lanes, and on-street parking lanes on both sides of the street. South-bound at Park Road, the parking lane converts to a dedicated right-turn lane.
- The Kenyon Street / Park Road / 14th Street intersection should be reconfigured to improve pedestrian safety. West-bound traffic at the Park Road / 14th Street intersection will permit a right-turn only northbound onto 14th Street. Kenyon Street will be reduced to two travel lanes at 14th Street.
- North-bound travel on 14th Street at the Park Road / Kenyon Street intersection will have a dedicated left-turn lane and one through travel lane which extends north to Monroe.
- The Irving Street / 14th Street intersection is also improved by narrowing the roadbed to provide a south-bound left turn lane and one through travel lane.



Plan

The curb areas shown in red illustrate recommended curb extensions

Scale: 1" = 150'



14th Street NW - Typical Street Sections
Core Area between Irving Street NW and Park Road NW illustrated
At Midblock (above) and at Curb Extension (below)

Scale: 1" = 20'

Focus Area #3

Gateway Improvements

- 3a 14th Street and Columbia Road,
- 3b 14th Street and Harvard Street
 - Provide curb extensions with special paving
 - Define crosswalks with special pavement or marking
 - Provide ornamental trees at intersections
 - Add public art, lighting, trash receptacles and benches at curb extensions.

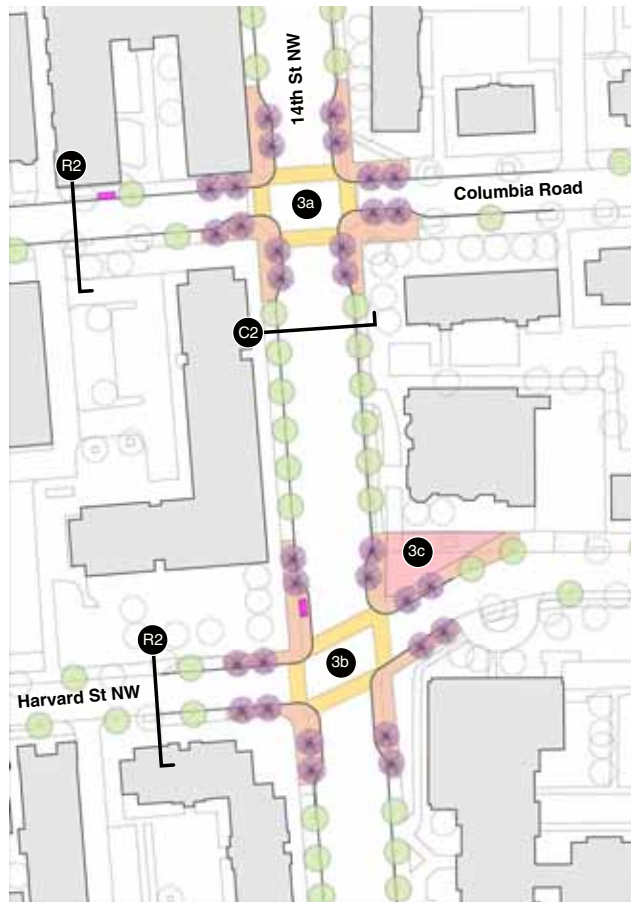
R.O.W. Improvements

- C2 14th Street,
- R2 Columbia Road, Harvard Street

Refer to Street Prototypes for street trees, paving, lighting and furnishing information.

Special Design Opportunities

- 3c Urban League
 - Improve parcel frontage and streetscape in coordination with gateway development.



Focus Area #4

Gateway Improvements

- 4a 14th Street and Girard Street
 - Provide curb extensions with special paving
 - Define crosswalks with special pavement or marking
 - Provide ornamental trees at intersections
 - Add public art, lighting, trash receptacles and benches at curb extensions.

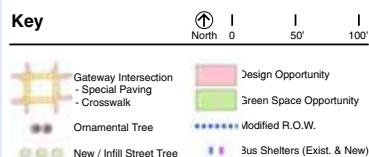
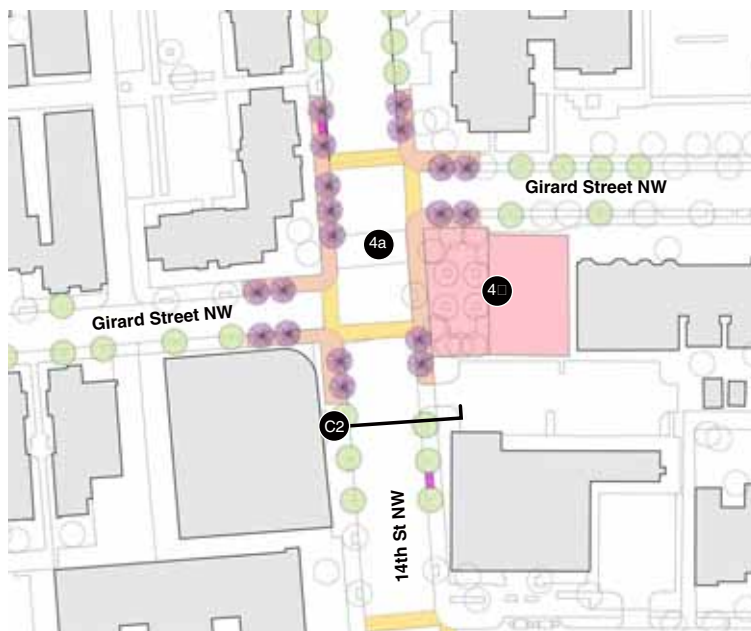
R.O.W. Improvements

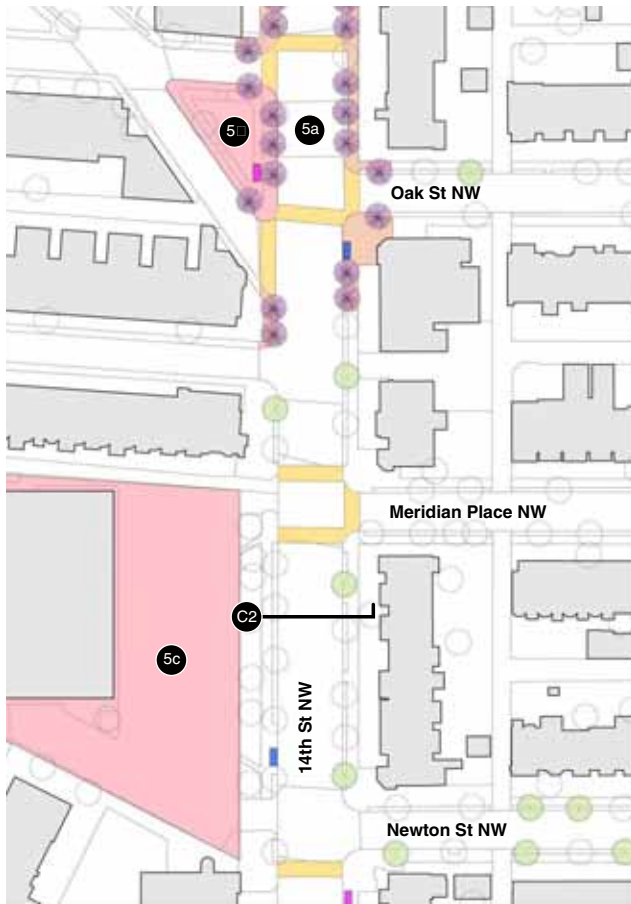
- C2 14th Street,
- R2 Girard Street

Refer to Street Prototypes for street trees, paving, lighting and furnishing information.

Special Design Opportunities

- 4i Park & Basketball Court
 - Repair game tables, lighting, walls and paving surfaces
 - Improve landscape
 - Incorporate public art
 - Coordinate improvements with C.I.P.





Focus Area #5

Gateway Improvements

5a 14th Street and Oak Street

- Provide curb extensions with special paving
- Define crosswalks with special pavement or marking
- Provide ornamental trees at intersections
- Add public art, lighting, trash receptacles and benches at curb extensions.

R.O.W. Improvements

C2 14th Street NW

Refer to Street Prototypes for street trees, paving, lighting and furnishing information.

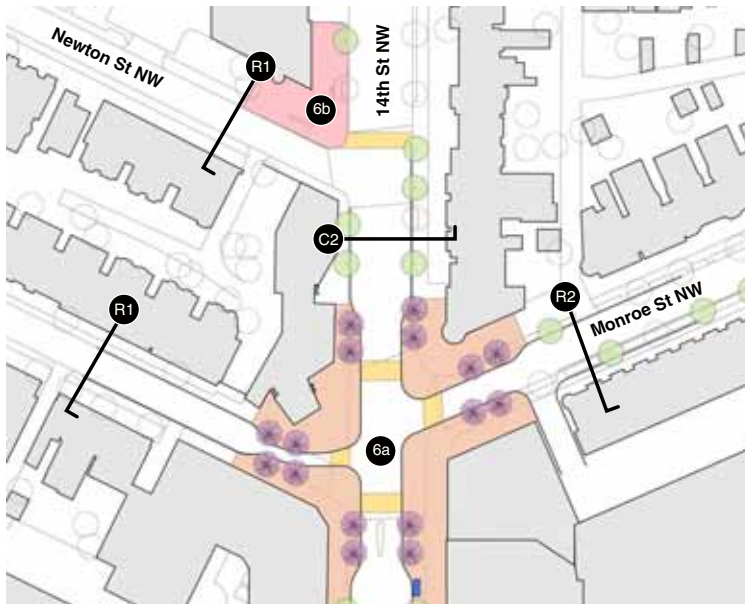
Special Design Opportunities

5c Park at Oak Street

- Improve landscaping, lighting, seating, furnishings and pavement.
- Incorporate public art.

5c Existing Giant Parcel

- Redevelop existing site to provide community resource following the opening of new Giant Store on Park Road.



Focus Area #6

Gateway Improvements

6a 14th Street and Monroe Street

- Provide curb extensions with special paving
- Define crosswalks with special pavement or marking
- Provide ornamental trees at intersections
- Add public art, lighting, trash receptacles and benches at curb extensions.

R.O.W. Improvements

C2 14th Street NW,

R1 Monroe St., Newton St. (west of 14th St),

R2 Monroe St. (east of 14th St.)

Refer to Street Prototypes for street trees, paving, lighting and furnishing information.

Special Design Opportunities

6b Fire Station Plaza

- Improve landscaping, lighting, seating, furnishings and pavement
- Incorporate public art.

Focus Area #7

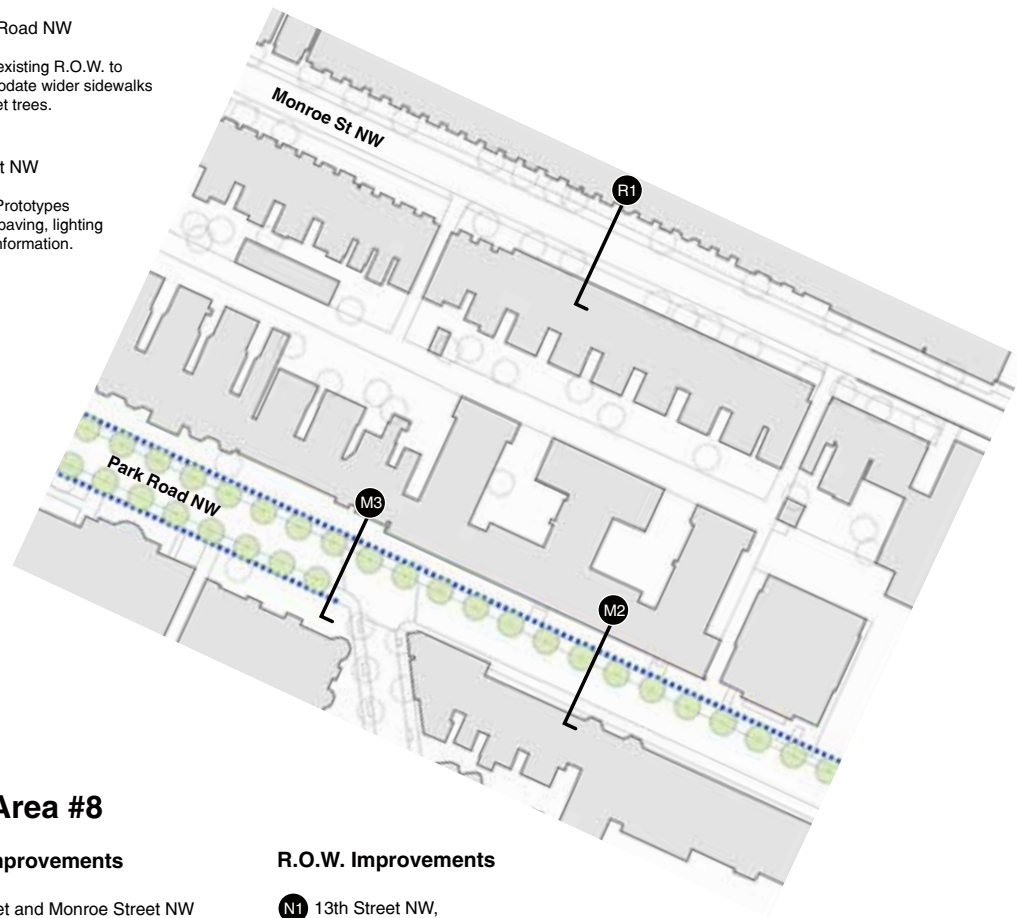
R.O.W. Improvements

M2 M3 Park Road NW

- Expand existing R.O.W. to accommodate wider sidewalks and street trees.

R1 Monroe St NW

Refer to Street Prototypes for street trees, paving, lighting and furnishing information.



Focus Area #8

Gateway Improvements

8a 13th Street and Monroe Street NW

- Provide curb extensions with special paving
- Define crosswalks with special pavement or marking
- Provide ornamental trees at intersections
- Add public art, lighting, trash receptacles and benches at curb extensions.

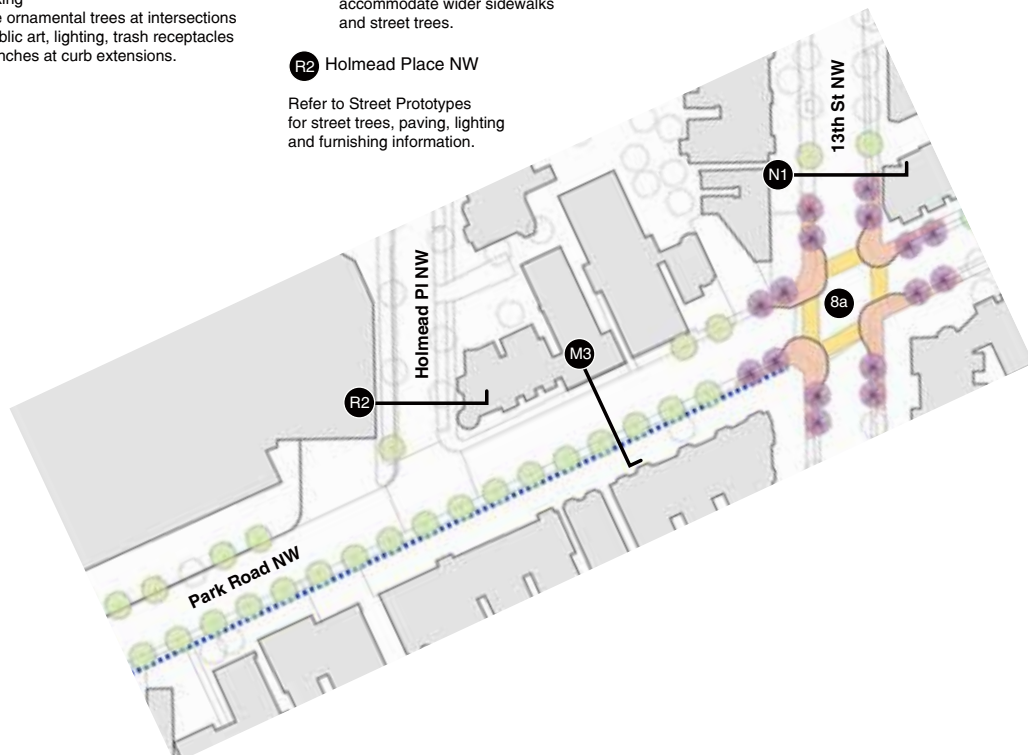
R.O.W. Improvements

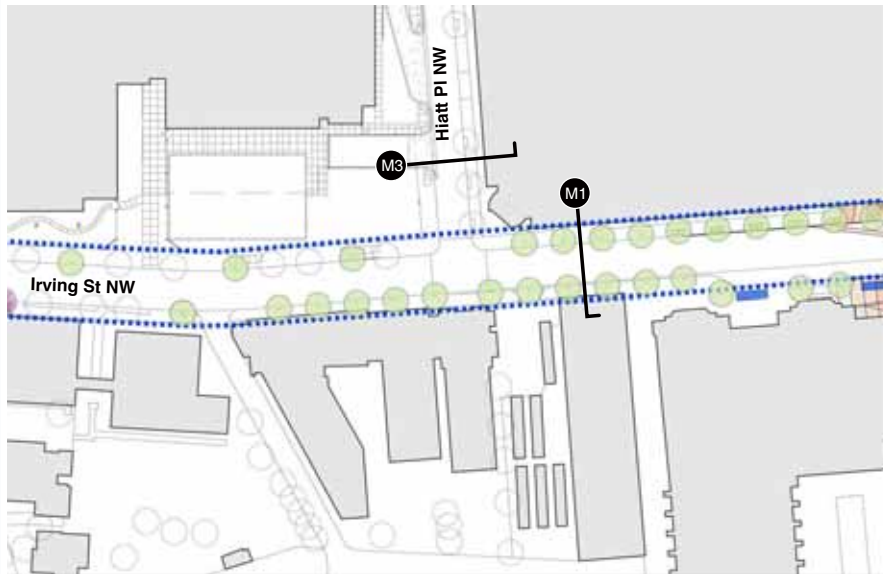
N1 13th Street NW,

- #### M3 Park Road NW,
- Expand existing Park Road R.O.W. to accommodate wider sidewalks and street trees.

R2 Holmead Place NW

Refer to Street Prototypes for street trees, paving, lighting and furnishing information.



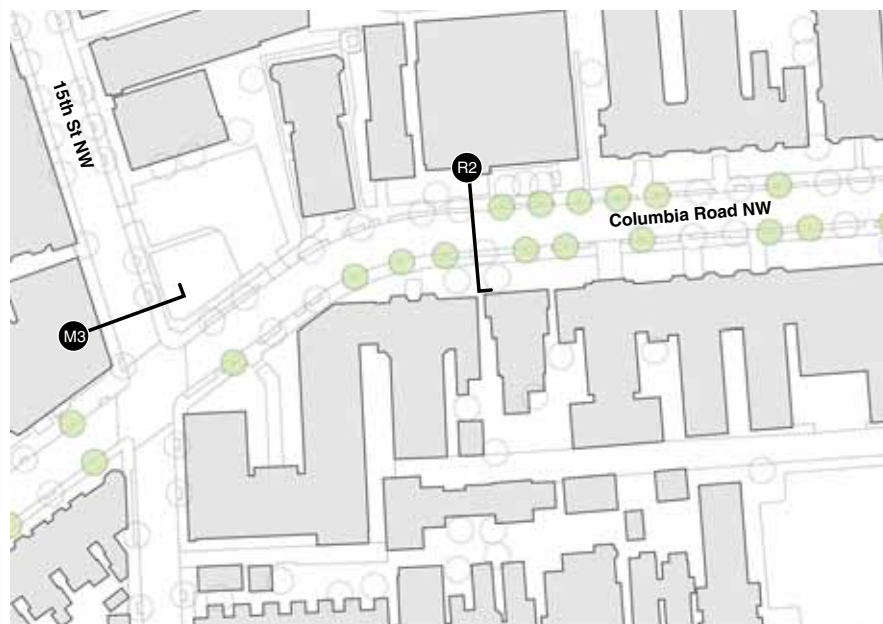


Focus Area #9

R.O.W. Improvements

- M3 Hiatt Place NW,
- M1 Irving Street NW

Refer to Street Prototypes for street trees, paving, lighting and furnishing information.



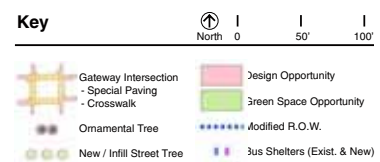
Focus Area #10

R.O.W. Improvements

- M3 15th Street NW,
- R2 Columbia Road NW

Refer to Street Prototypes for street trees, paving, lighting and furnishing information.

Key



Focus Area #11

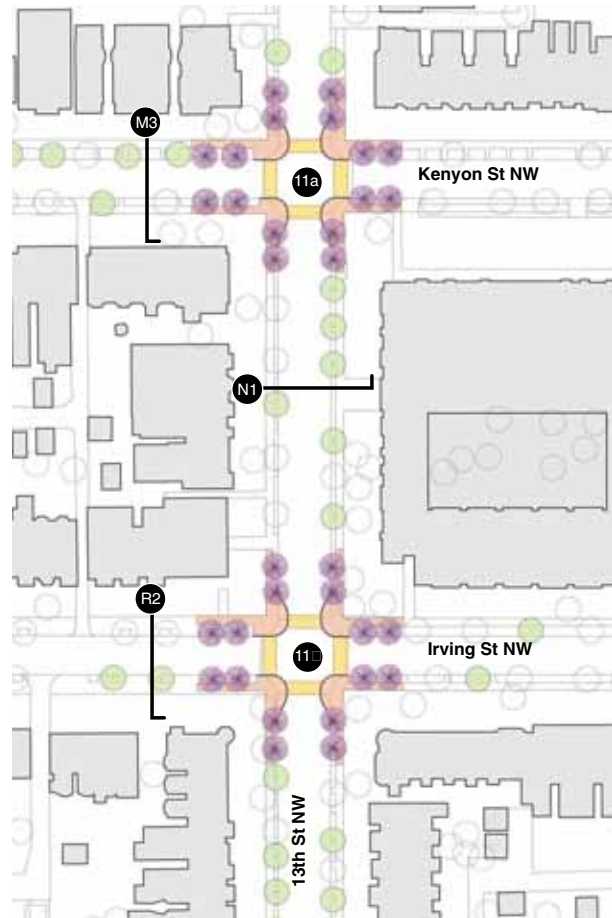
Gateway Improvements

- 11a 13th Street and Kenyon St NW,
- 11 13th Street and Irving St NW
 - Provide curb extensions with special paving
 - Define crosswalks with special pavement or marking
 - Provide ornamental trees at intersections
 - Add public art, lighting, trash receptacles and benches at curb extensions.

R.O.W. Improvements

- N1 13th Street NW,
- M3 Kenyon Street NW,
- R2 Irving Street NW

Refer to Street Prototypes for street trees, paving, lighting and furnishing information.



Focus Area #12

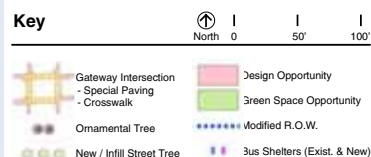
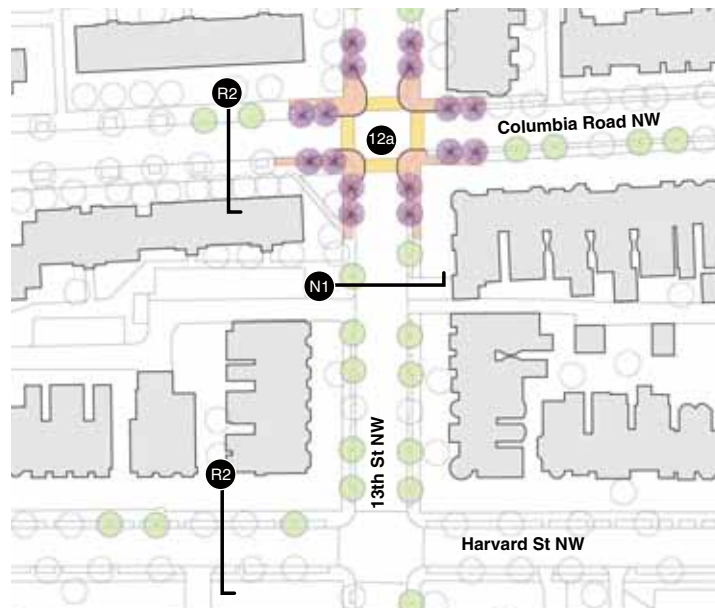
Gateway Improvements

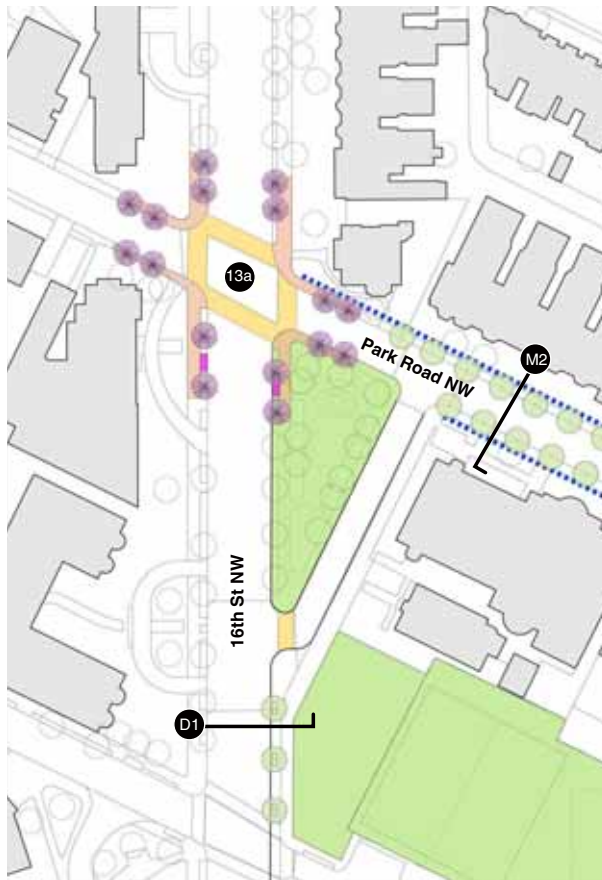
- 12a 13th Street and Kenyon St NW
 - Provide curb extensions with special paving
 - Define crosswalks with special pavement or marking
 - Provide ornamental trees at intersections
 - Add public art, lighting, trash receptacles and benches at curb extensions.

R.O.W. Improvements

- N1 13th Street NW,
- R2 Columbia Road NW, Harvard St NW

Refer to Street Prototypes for street trees, paving, lighting and furnishing information.





Focus Area #13

Gateway Improvements

13a 16th Street and Park Road

- Provide special paving at intersection sidewalks
- Define crosswalks with special pavement or marking
- Provide ornamental trees at intersections
- Add public art, lighting, trash receptacles and benches at curb extensions.

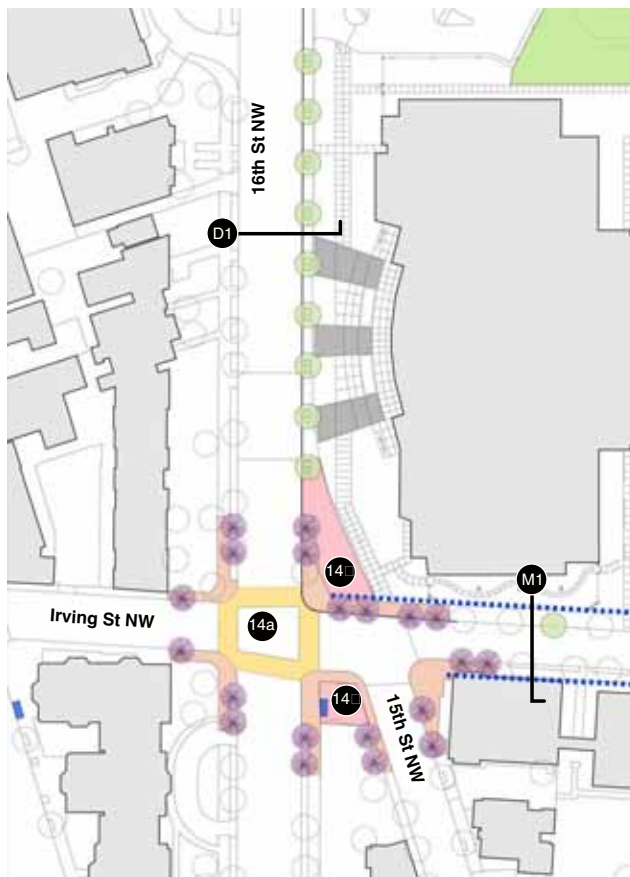
R.O.W. Improvements

D1 16th Street NW,

M1 Park Road NW

- Expand existing Park Road R.O.W. to accommodate wider sidewalks and street trees.

Refer to Street Prototypes for street trees, paving, lighting and furnishing information.



Focus Area #14

Gateway Improvements

14a 16th Street and Irving Street

- Provide special paving at intersection sidewalks
- Define crosswalks with special pavement or marking
- Provide ornamental trees at intersections
- Add public art, lighting, trash receptacles and benches at curb extensions.

R.O.W. Improvements

D1 16th Street NW,

M1 Irving Street NW

Refer to Street Prototypes for street trees, paving, lighting and furnishing information.

Special Design Opportunities

14: Parcels at 15th Street NW Intersection

- Coordinate closure of 15th Street north of Irving St. with gateway development
- Improve landscape, lighting, paving and public art.

Focus Area #15

Gateway Improvements

15a 16th Street, Columbia Road and Harvard Street

- Provide special paving at intersection sidewalks
- Define crosswalks with special pavement or marking
- Incorporate landscaping and public art at concrete medians along 16th St. NW.
- Provide ornamental trees at intersections
- Add lighting, trash receptacles and benches at intersection.

R.O.W. Improvements

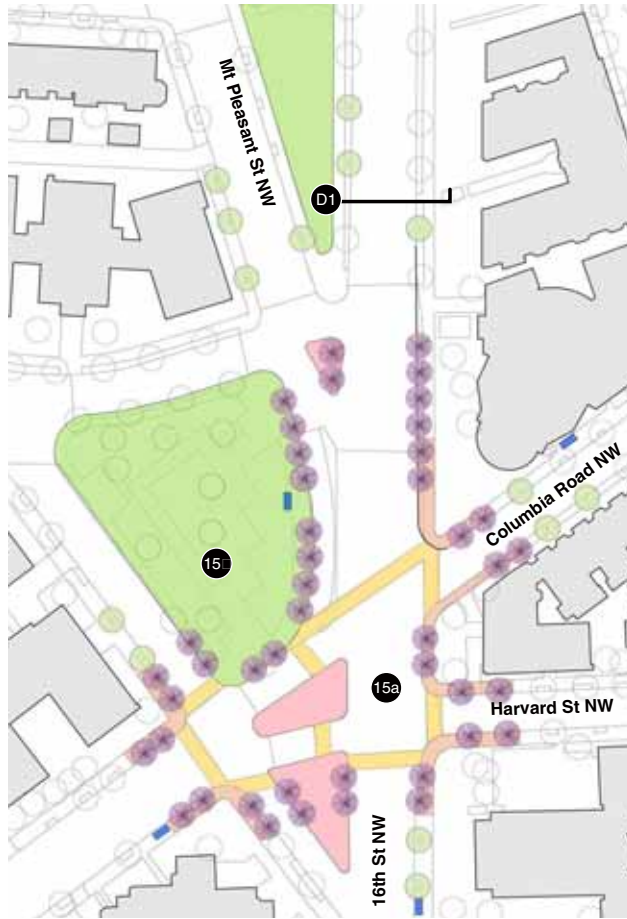
D1 16th Street NW

Refer to Street Prototypes for street trees, paving, lighting and furnishing information.

Special Design Opportunities

15 Park

- Improvements to the park should include enhanced landscaping, seating, lighting and development of a significant public art feature.



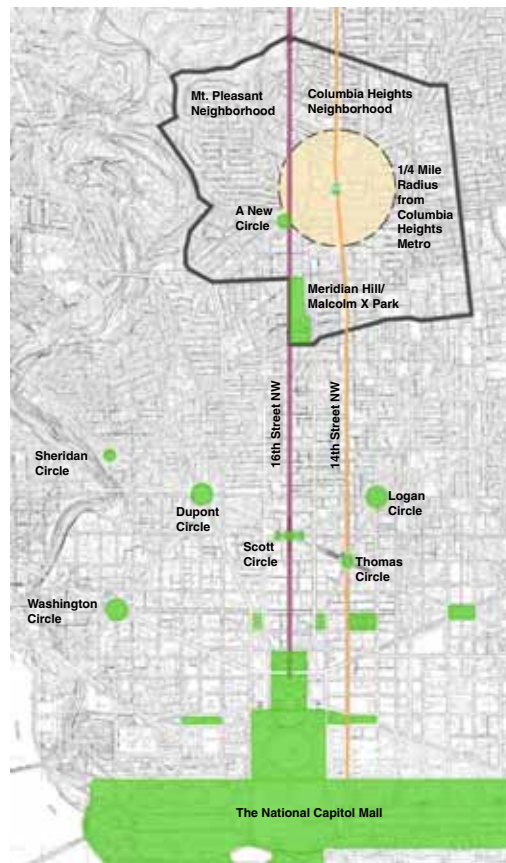
Focus Area #15

Long Range Improvements

Improve Mt. Pleasant and Columbia Heights neighborhood connections by making significant pedestrian and vehicular traffic improvements at the park adjacent to 16th St NW.

Improvements to the park should consider opportunity to extend the historic L'Enfant plan north on 16th by developing a traffic circle / tunnel. The new circle would be similar in size to Thomas Circle.

Improvements to the park should include enhanced landscaping, seating, lighting and development of a significant public art feature.



Street Character Framework and Prototypes

The following pages illustrate the recommended designs for each street right-of-way in the neighborhood. The designs are arranged by a hierarchy of street character and scale, from the 14th Street Commercial Core Primary Street – in sequence – to the Secondary Mixed Use and Residential Streets. The recommended streetscape design of each street are presented as 'Street Prototypes' which use a typical curbside condition of each street type to illustrate the standard.



Sidewalk Zones

The sidewalk zones, summarized below and illustrated to the right, recommend the preferred dimensions for each sidewalk based on street type and adjacent uses.

Building Frontage Zone (BFZ):

Area of sidewalk directly abutting buildings and accommodates cafe tables, building entries and minor building projections such as meters and downspouts.

Dimensions: Varies by Street R.O.W. and Building Setback

Through Pedestrian Zone (TPZ):

Space for through-pedestrian traffic, unencumbered by any obstructions.

Dimensions: Commercial Streets - Minimum 10' Recommended
Mixed-Use Streets - Minimum 9' Recommended
Residential Streets - 5' to 6'

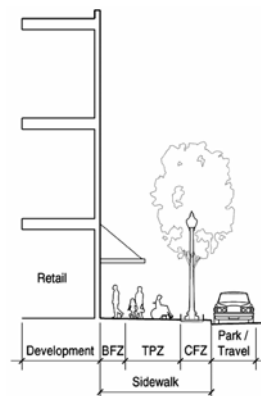
Curb & Furnishing Zone (CFZ):

Space for furnishings including landscape, signage, lighting, furniture, transit facilities, and special elements, includes width of curb and may include clear space for pedestrian access to transit or on-street parking.

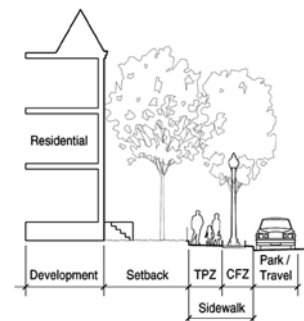
Pervious treatment is recommended in furnishing zone at tree wells and may include grass, groundcover or sand-set, pervious pavers. Rigid pavement can be used in limited areas with intense pedestrian use. All permanent vertical objects should be set back 18 inches minimum from the face of curb.

Dimensions: Tree Well - 4' Minimum Width
Curb with Pedestrian Access - 1'-6"
CFZ - Minimum 6' Recommended

Typical Sidewalk Zones



Street with Adjacent Commercial Uses



Street with Adjacent Residential Uses

C1 Commercial Core Primary Street

14th St. NW (Core Area, between Irving and Monroe)



Key Plan

Sidewalk Treatment

Type: Special Paving - Refer to Civic Plaza section for scoring pattern

Street Tree

Species: Pin Oak - Replace / infill with species to be verified with City horticulturalist
Spacing: 40' on center
Tree Well: Minimum 4' x 10' with ground cover, pervious paving or tree grates where necessary

Street Lighting

Type: Single Globe No. 18
(Replace existing Cobra Head)
Color: Custom
Spacing: 40' on center typical
Pattern: Opposite across street

Bus Shelter

Type: Custom

Bench

Type: City Standard
Location: Intersections

Trash Receptacle

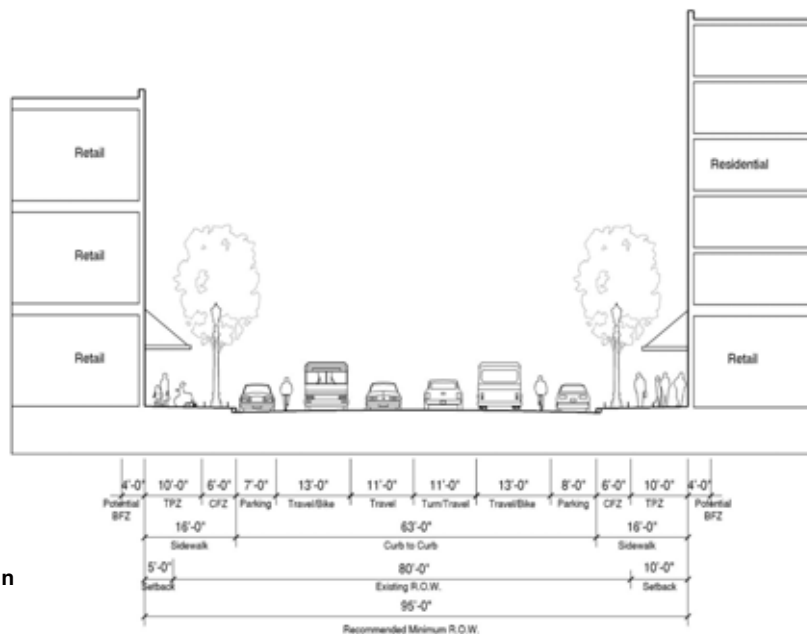
Type: City Standard
Location: 4 per block face, minimum
Color: Custom
Plaque: Custom

Bicycle Storage

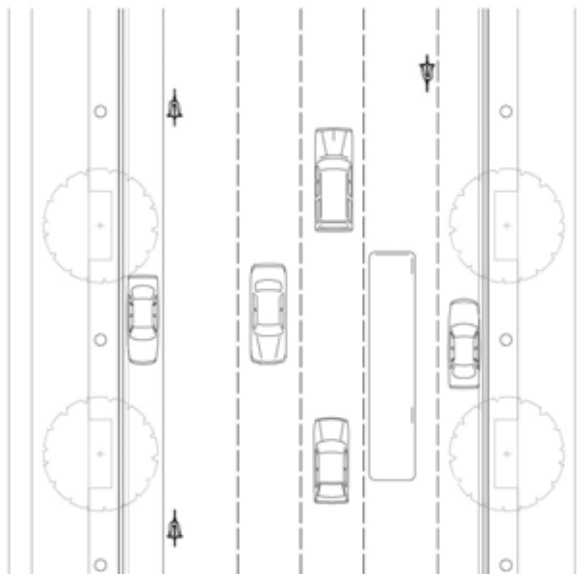
Rack: City Standard
Locker: Metro Standard

Refer to "Streetscape Furnishing" section for additional information.

Typical Section



Typical Plan



C2 Commercial Core Primary Street

14th St. NW (north and south of Core area)

Sidewalk Treatment

Type: Scored Concrete, 3' x 3' Typ.

Street Tree

Species: Pin Oak - Replace / infill with species to be verified with City horticulturalist
Spacing: 30'-50' on center
Tree Well: Minimum 4' x 10' with ground cover, pervious paving or tree grates where necessary

Street Lighting

Type: Single Globe No. 18 (Replace existing Cobra Head)
Color: Custom
Spacing: 60' on center typical
Pattern: Staggered across street

Bus Shelter

Type: Custom

Bench

Type: City Standard
Location: Gateway intersections

Trash Receptacle

Type: City Standard
Location: Gateway intersections
Color: Custom
Plaque: Custom

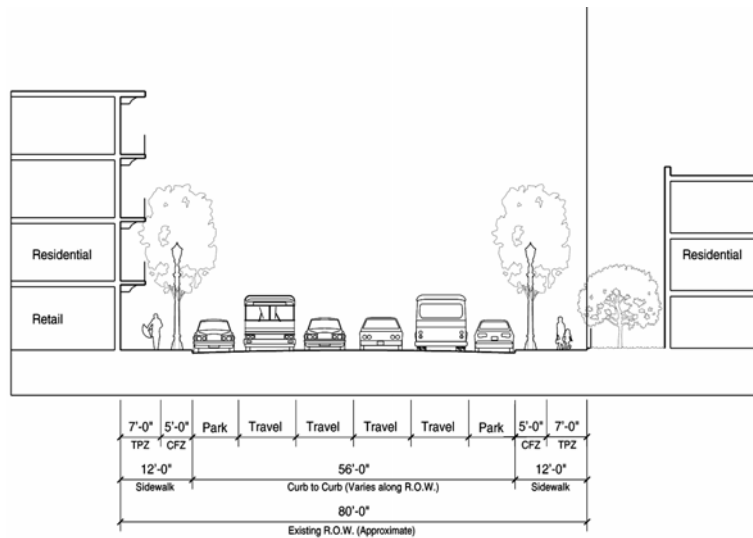
Bicycle Storage

Rack: Rack

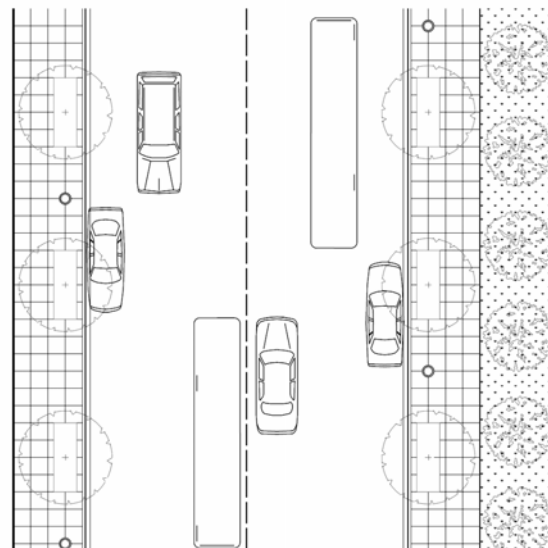
Refer to "Streetscape Furnishing" section for additional information.



Key Plan



Typical Section



Typical Plan

D1 District Connector 16th St. NW



Key Plan

Sidewalk Treatment

Type: Scored Concrete, 3' x 3' typical

Street Tree

Species: Greenspire Littleleaf Linden
Spacing: 30'-50' on center
Tree Well: Minimum 4' x 10' with ground cover, pervious paving or tree grates where necessary

Street Lighting

Type: Twin-20 Globe
(Replace existing Cobra Head)
Color: Custom
Spacing: 80' on center typical
Pattern: Opposite across street

Bus Shelter

Type: City Standard

Bench

Type: City Standard
Location: Gateway intersections

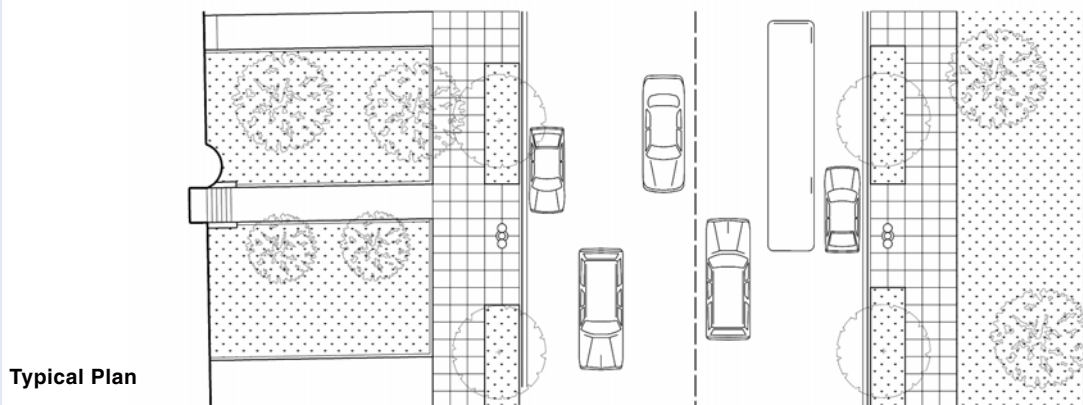
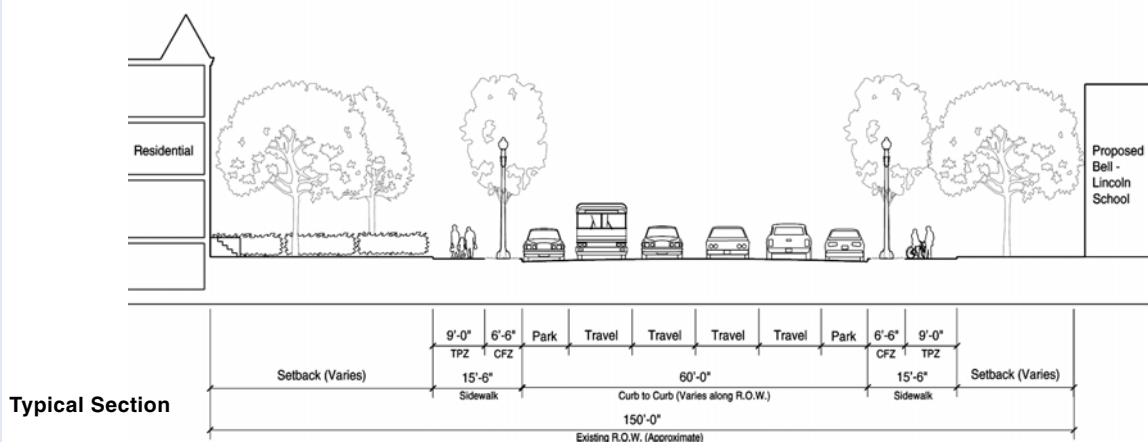
Trash Receptacle

Type: City Standard
Location: Gateway Intersections
Color: Custom
Plaque: Custom

Bicycle Storage

Rack: City Standard

Refer to "Streetscape Furnishing" section for additional information.



N1 Neighborhood Connector

13th St. NW

Sidewalk Treatment

Type: Scored Concrete, 3' x 3' typical

Street Tree

Species: Norway Maple - Replace / infill with species to be verified with City horticulturalist
Spacing: 30'-50' on center
Tree Well: Minimum 4' x 10' with ground cover, pervious paving or tree grates where necessary

Street Lighting

Type: Single Globe No. 18 (Replace existing Cobra Head)
Color: Custom
Spacing: 60' on center typical
Pattern: Opposite across street

Bench

Type: City Standard
Location: Gateway Intersections

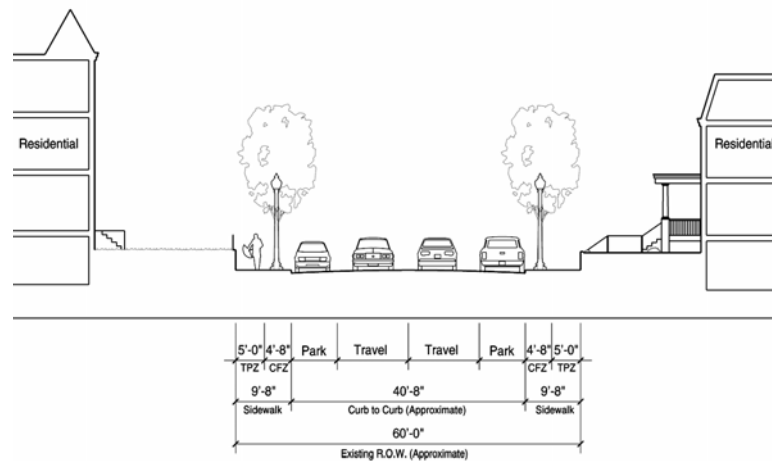
Trash Receptacle

Type: City Standard
Location: Gateway Intersections
Color: Custom
Plaque: Custom

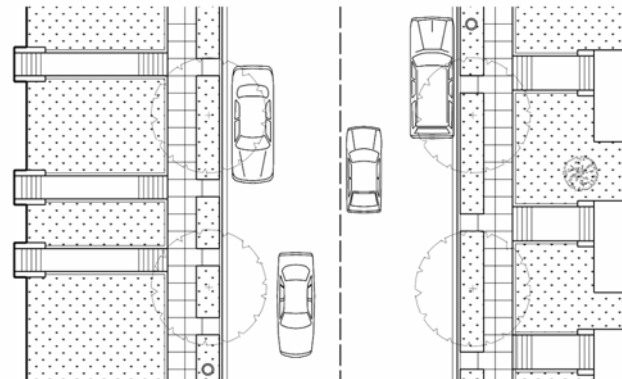
Refer to "Streetscape Furnishing" section for additional information.



Key Plan



Typical Section



Typical Plan

M1, M2 Mixed-Use Secondary Street

M1 - Irving St. NW (west of 14th Street NW)

M2 - Park Road NW (west of 14th Street NW)



Key Plan

Sidewalk Treatment

Type: Scored Concrete, 3' x 3' typical

Street Tree

Species: Irving St - Scarlet Oak
Park Road - Willow Oak

Spacing: 30'-50' on center

Tree Well: Minimum 4' x 10' with ground cover, pervious paving or tree grates where necessary

Street Lighting

Type: Single Globe No. 15
(Replace existing Cobra Head)

Color: Custom

Spacing: 60' on center typical

Pattern: Staggered across street

Bench

Type: City Standard

Location: Gateway Intersections

Trash Receptacle

Type: City Standard

Location: Gateway Intersections

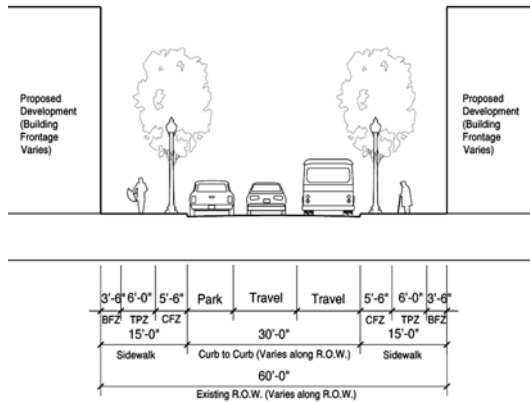
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Plaque: Custom

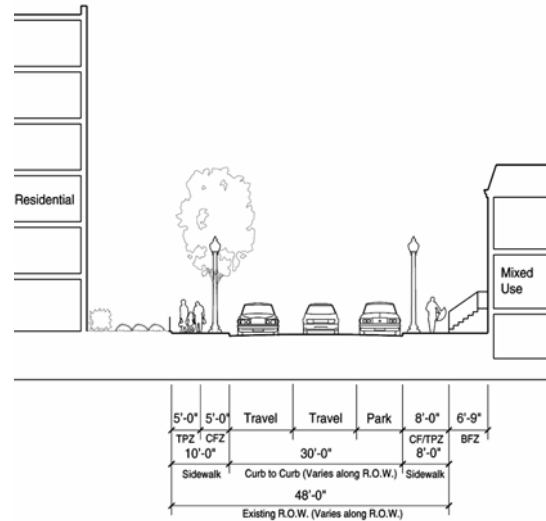
Bicycle Storage

Rack: City Standard

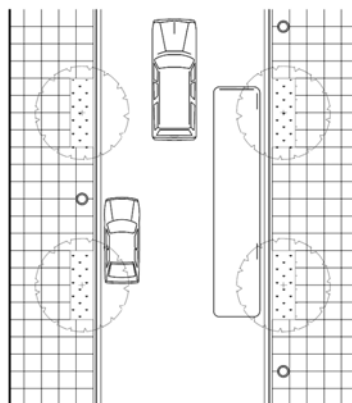
Refer to "Streetscape Furnishing" section for additional information.



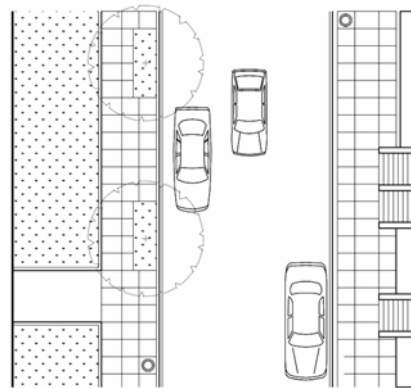
M1 - Typical Section



M2 - Typical Section



M1- Typical Plan



M2- Typical Plan

M3 Mixed-Use Secondary Street

Irving St. NW (illustrated), Park Road NW (east of 14th Street)

Columbia Road, NW, Kenyon St. NW, Hiatt Place NW, 15th St. NW, Pine St. NW

Sidewalk Treatment

Type: Scored Concrete, 3' x 3' typical

Street Tree

Species: Park Road - Willow Oak
Kenyon Street, 15th Street
- Pin Oak - Replace / infill with
species to be verified with City
horticulturalist
Hiatt Place, Pine St - Replace /
infill with species to be verified
with City horticulturalist

Spacing: 30'-50' on center

Tree Well: Minimum 4' x 10' with ground
cover, pervious paving or tree
grates where necessary

Street Lighting

Type: Single Globe No. 15
(Replace existing Cobra Head)

Color: Custom

Spacing: 60' on center typical

Pattern: Opposite across street

Bench

Type: City Standard

Location: Gateway Intersections

Trash Receptacle

Type: City Standard

Location: Gateway Intersections

Color: Custom

Plaque: Custom

Bicycle Storage

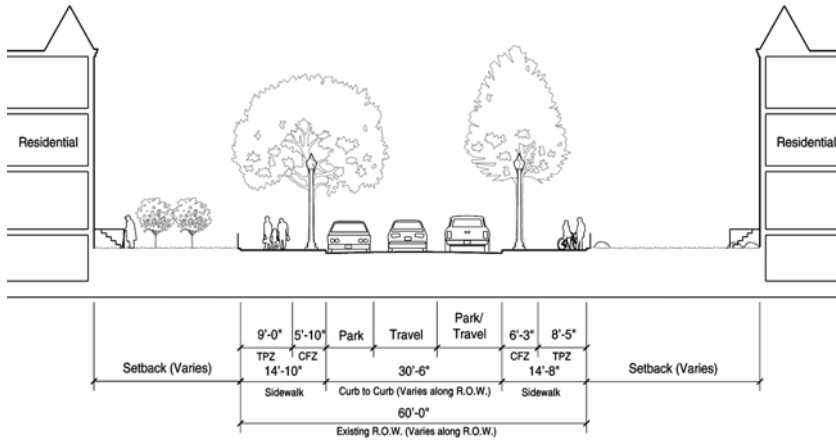
Rack: City Standard



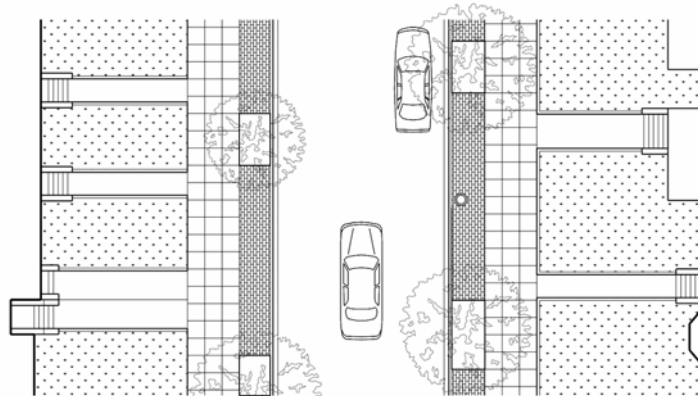
Key Plan

Refer to "Streetscape Furnishing" section
for additional information.

Typical Section



Typical Plan



R1 Residential Secondary Street

Monroe St. NW (illustrated), Newton St. NW (west of 14th)



Key Plan

Sidewalk Treatment

Type: Scored Concrete, 3' x 3' typical

Street Tree

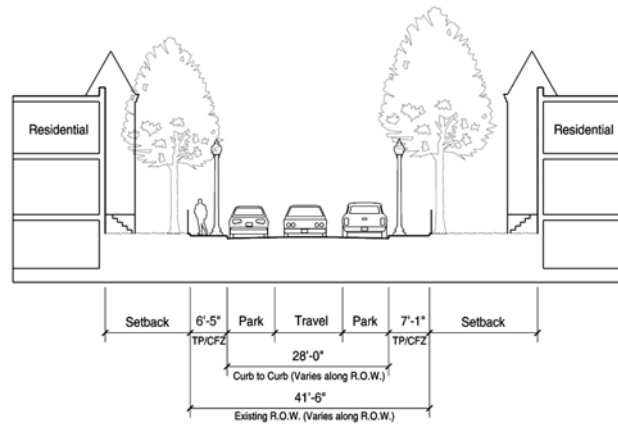
Species: Monroe Street - London Planetree
 Newton Street - Norway Maple
 - Replace / infill with species to be verified with City horticulturalist
 Spacing: 30'-50' on center
 Tree Well: Minimum 4' x 10' with ground cover, pervious paving or tree grates where necessary

Street Lighting

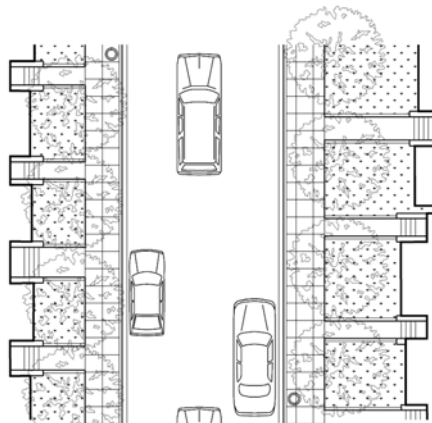
Type: Single Globe No. 15
 (Replace existing Cobra Head)
 Color: Custom
 Spacing: 60' on center typical
 Pattern: Staggered across street

Refer to "Streetscape Furnishing" section for additional information.

Typical Section



Typical Plan



R2 Residential Secondary Street

Harvard St. NW (illustrated), Columbia Road NW, Girard St. NW
Monroe St. NW, Holmead Place NW, Newton St. NW (east of 14th)

Sidewalk Treatment

Type: Scored Concrete, 3' x 3' typical

Street Tree

Species: Columbia Road - London Planetree
Girard Street - Red Maple
Monroe Street - London Planetree
Harvard Street, Newton Street
- Pin Oak and Norway Maple,
Replace / infill with species to be
verified with City horticulturalist

Spacing: 30-50' on center

Tree Well: Minimum 4' x 10' with ground
cover, pervious paving or tree
grates where necessary

Street Lighting

Type: Single Globe No. 15
(Replace existing Cobra Head)

Color: Custom

Spacing: 60' on center typical

Pattern: Staggered across street

Bus Shelter

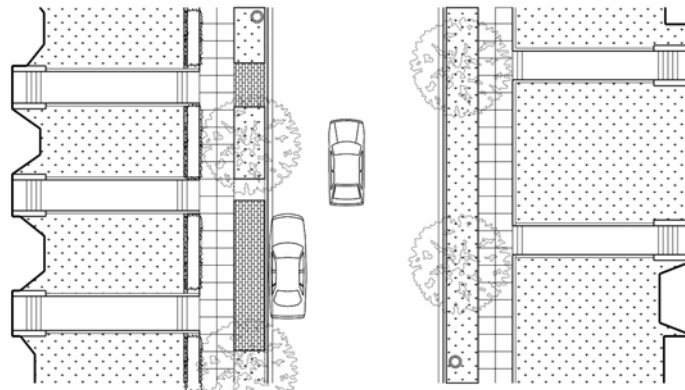
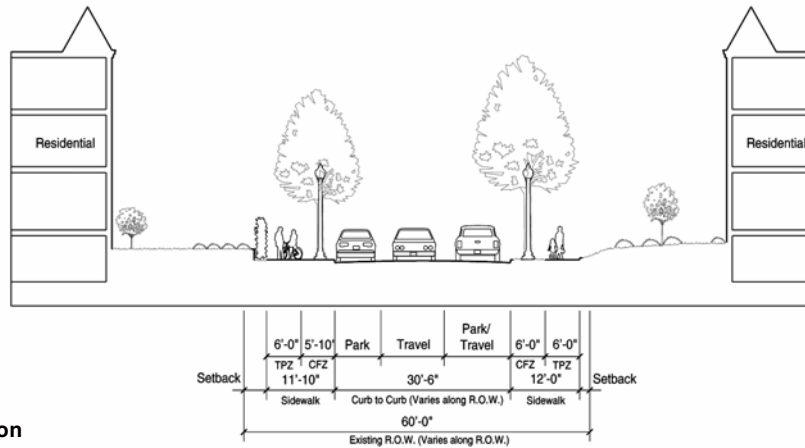
Type: Custom near 14th Street

Location: Columbia Road NW

Refer to "Streetscape Furnishing" section
for additional information.



Key Plan



Streetscape Furnishing Design

The Design of Streetscapes for Columbia Heights is derived from the signature qualities of the 'core area'. Streetscape elements include the three different scales of the standard DC ornamental street light poles, to be extended typically throughout the neighborhood, but placed at slightly more dense spacing in the 'core area'. Bench seating is selected from the DC standards. Bus Shelters are proposed to be custom-designed in the 'core area', but DC standard outside the 'core area'.

The color of the furnishings should be uniform throughout the neighborhood. The design sub-committee recommended the use of a custom color for all furnishings with a preference for a dark or forest green. Final color selection should be coordinated with the development of the Civic Plaza.



DC Standards



DC Standard - Single Globe 15, 18 and Twin 20
Note: Custom color to be used for all lights

Street Lights

Types: DC Standard
Single Globe - 15'-1" Ht.
Single Globe - 18'-1" Ht.
Twin - 20'-0" Ht.
Locations: Refer to Street Prototypes
Color: Custom

Source: Downtown DC Streetscape Plan



DC Standard -
Landscape Forms Plainwell



DC Standard -
Victor Stanley C-10

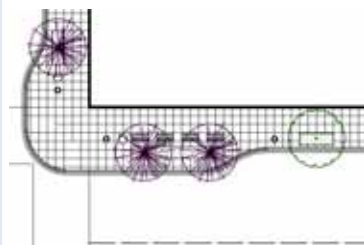


Example of bench with
custom color at area with
special paving

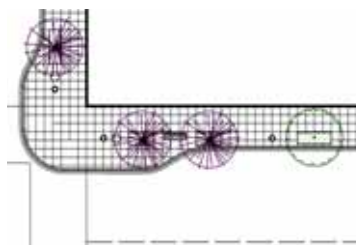
Benches

Types: Downtown DC Standard
Custom may be allowed
Locations: Gateway Intersections
Color: Custom

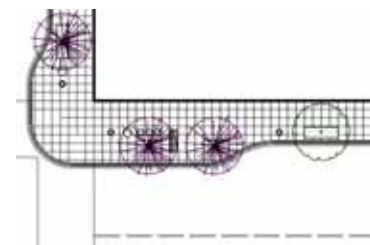
Source: Downtown DC Streetscape Plan



Recommended Bench Location -
Adjacent to Street Trees



Recommended Bench Location -
Between Street Trees



Recommended Bench Location -
Perpendicular to Street



Example of Existing Trash Receptacles



Recommended

Trash and Recycling Receptacles

Types: DC Standard
Model - Victor Stanley
Locations: Refer to Street Prototypes
Color: Custom
Plaque: Custom

Bike Racks

Types: DC Standard Posts
Custom
Locations: Refer to Street Prototypes
Color: Custom



DC Standard



Examples of Custom Racks

Bike Lockers

Types: Coordinate with Metro
Locations: Metro Entrances
Color: Custom



Metro Standard

Tree Wells & Guards

Tree Well: 4' x 10' Minimum
Tree Guard: Metal Hoops recommended
on Commercial, Connector and
High Pedestrian Activity Streets
Metal or Brick on Residential
Streets
Color: Custom in 'core area'



Examples of recommended tree well planting and guards

Newspaper Boxes

Types: Consolidated Newspaper Boxes
Locations: Metro Entrances and Gateway
Intersections - Restricted to
Furnishing Zone
Color: Custom



Existing newspaper boxes



Recommended newspaper boxes

Drinking Fountains

Locations: Civic and Metro Plaza
Type: To be determined with design of
plazas
Color: Stainless Steel or Prefinished
Metal with Custom Color



Drinking fountain example

Special Signage

Potential Types:
DC Standard Information Sign
Renovated Fire Call Station
Custom Columbia Heights Signs
Language: English, Spanish, Vietnamese

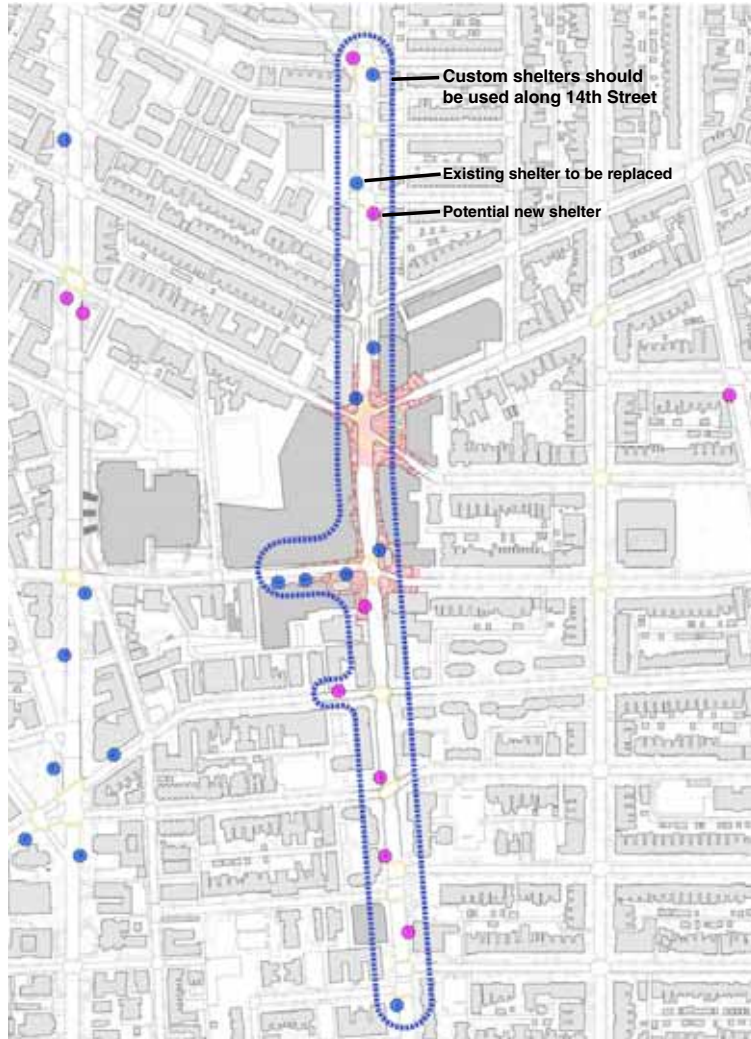


Example History Sign



Existing Fire Call Stations

Bus Shelters

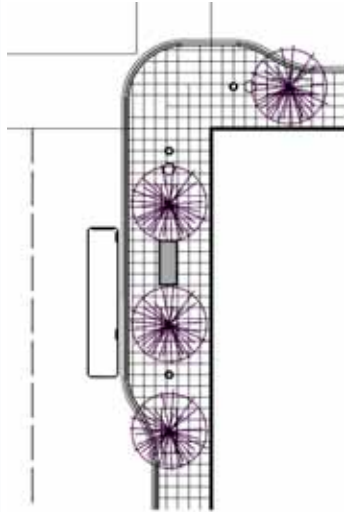


Design Criteria for Custom Shelters on 14th Street

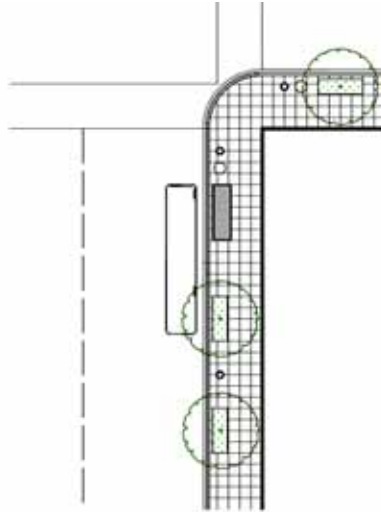
Shelters should:

- be an example of civic architecture
- be coordinated with the neighborhood color scheme
- integrate public art
- provide seating and leaning rails
- include lighting
- provide an overhead cover while maximizing access from the sidewalk
- minimize advertising
- include opportunities for neighborhood signage
- have the potential to be adapted for real-time signage

Recommendations



Recommended plan of bus shelter at curb extension



Recommended plan of bus shelter at typical curb condition



Example of potential custom shelter to be refined and installed along 14th Street which incorporates public art, leaning rails and is open to the sidewalk

Existing Shelters



Existing neighborhood bus shelter in front of the Tivoli Theater



Existing neighborhood bus shelter in front of the Metro Station



Existing downtown bus shelter - Proposed typical shelter, revised to be smaller, to be used throughout Columbia Heights at bus stops other than those along 14th St.

Public Art Framework

Background

Through an open invitation process, the DC Commission on the Arts and Humanities sought out and selected local artists to work in collaboration with Zimmer Gunsul Frasca, the DC Office of Planning, the Arts Commission, and the Project Design Sub-Committee as members of the Columbia Heights Design Team in the design of the Civic Plaza and 'core area' of the Public Realm Framework.

The offering is a two-part process which will lead to the award of one or more commissions for public art installations to be located in the heart of Columbia Heights. The goal of including artists in the design process has been to ensure that the public artworks will capture and integrate the distinct and unique character of the community and be incorporated within the Public Realm Framework.

The selection committee, including representatives from each of the groups listed above, reviewed 28 applications before selecting Jann Rosen-Queralt and Steven Weitzman, two artists with extensive backgrounds in public art commissions and design team projects.

Ms. Rosen-Queralt and Mr. Weitzman joined the Design Team in early July 2003. Over the course of their three month participation on the design team, the artists conducted research within the community to identify specific opportunities to integrate "place making" public artworks in civic spaces and streetscapes; held workshops with the Multicultural Community Service, a local youth group; met with the Design Team on numerous occasions to integrate their proposals into the Framework Plan; assisted with development of custom designs for artistic enhancements to standard streetscape elements; gave design presentations with the Design Team at public meetings and workshops; and proposed designs for site-specific artworks. Pending design approval, the DC Commission on the Arts and Humanities will fund \$100,000 of site-specific artwork.

Summary

The diagram to the right identifies the locations in the neighborhood where 'public art' should be integrated with the public realm.

The specific art proposals are included in Appendix 1.

Jann Rosen-Queralt's proposals:

- Plaza paving pattern for the public fountain consisting of a series of maps which describe the region's watersheds and historic development
- Community portraits and masks represented in a series of interactive panels located throughout the neighborhood
- Collection of 'Community Wishes' coordinated with development of mask panels and community events
- The Columbia Heights chair would be a specially designed set of chairs for use in and around the plaza.

Steven Weitzman's proposals:

- Bollard design with relay controlled lights placed at 20' intervals on the east and/or west side of 14th Street in the furnishing zone of the sidewalk and along the plaza perimeter
- Mosaic treatment on the walls within the Civic Plaza bordering 14th Street and on the vertical walls of the lawn areas
- Tension-fabric structure design for five to nine specialty light standards for the Civic Plaza and Metro Plaza
- Lenticular wall panels incorporated into the facades of the DC USA project along 14th Street.



Public Art Framework

Plazas

- 1 Civic Plaza
- 2 Metro Station Plazas

Primary Corridor

- 3 14th Street NW and Irving Street NW

Private Development

- 4 DC-USA

Public Institutions

- 5 Bell-Lincoln School
- 6 Tubman Elementary School
- 7 Giant Store - Future Community Resource

Gateways

- 8 14th Street NW & Spring Road NW
- 9 14th Street NW & Oak Street NW
- 10 14th Street NW & Newton Street NW
- 11 16th Street NW & Park Road NW
- 12 Park Road NW & Sherman Road NW
- 13 16th Street NW & Irving Street NW
- 14 16th Street NW & Columbia Road NW
- 15 14th Street NW & Harvard Street NW
- 16 13th Street NW & Columbia Road NW
- 17 14th Street NW & Girard Street NW
- 18 14th Street NW & Florida Ave NW
- 19 16th St NW & Spring Road NW

3 Implementation



Preliminary Cost Estimates
Phasing Strategy
Management Program

Preliminary Construction Budget Estimates for Public Realm Elements in the 'Core Area'

Curbside Improvements - Development Projects

Phasing / Coordination I.D.	RLA Parcels	Construction Zone	Construction Area	Unit Cost*	Construction Costs	Construction Costs with 20% Contingency	Projected Developer Costs	Potential Funding Gap / TIF Contribution
			SF.	\$/SF.	(See Note 1)	\$\$\$	(See Note 2)	(See Note 3)
						\$\$\$	%	%
A	29	Southeast / 14th Frontage						
		Parcel 29a / Tivoli Theatre	18,020	\$20.00	\$360,400			
		Curb and Gutter	595	\$100.00	\$59,500			
		Parcel 29b / Giant Frontage	11,852	\$15.00	\$177,780			
		Curb and Gutter	290	\$100.00	\$29,000			
		Subtotals			\$626,680	\$752,016	\$376,008	\$376,008
B	Metro	METRO Parcel						
		Triangle Development	7,860	\$20.00	\$157,200			
		Curb and Gutter	320	\$100.00	\$32,000			
		Subtotals			\$189,200	\$227,040	\$113,520	\$113,520
C	38	14th St. / Park-to-Kenyon						
		Parcel 38 / Civic Plaza	11,288	\$100.00	\$1,128,800			
		Curb and Gutter	291	\$100.00	\$29,100			
		Subtotals			\$1,157,900	\$1,389,480	\$0	\$1,389,480
D	15	14th St. / Kenyon-to-Irving						
		Parcel 15 / Donatelli & Klein	22,050	\$20.00	\$441,000			
		Curb and Gutter	790	\$100.00	\$79,000			
		Subtotals			\$520,000	\$624,000	\$312,000	\$312,000
	26	14th St. / Irving-to-Columbia Rd.						
		Parcel 26 / Donatelli & Klein	18,784	\$20.00	\$375,680			
		Curb and Gutter	537	\$100.00	\$53,700			
		Subtotals			\$429,380	\$515,256	\$257,628	\$257,628
E	27	14th St. / Park-to-Irving						
		Parcel 27 / Grid DC-USA	27,480	\$20.00	\$549,600			
		Curb and Gutter	1,280	\$100.00	\$128,000			
		Subtotals			\$677,600	\$813,120	\$406,560	\$406,560
F	24	14th St. / Monroe Ave. - North						
		Northwest / 14th Frontage						
		Parcel 24 / Dance Institute	6,782	\$15.00	\$101,730			
		Curb and Gutter	370	\$100.00	\$37,000			
		Subtotals			\$138,730	\$166,476	\$83,238	\$83,238
Subtotal			128,589	\$29.08	\$3,739,490	\$4,487,388	\$1,548,954	\$2,938,434

Curbside Improvements - Adjacent to Existing Development

Phasing / Coordination I.D.	Construction Zone	Construction Area	Unit Cost*	Construction Costs	Construction Costs with 20% Contingency	Projected Developer Costs	Potential Funding Gap / TIF Contribution
		SF.	\$/SF.	(See Note 1)	\$\$\$	(See Note 2)	(See Note 3)
					\$\$\$	%	%
A1	14th St. / Monroe-to-Park						
	Southwest / 14th Frontage						
	Riggs Bank Building	11,998	\$20.00	\$239,960			
	Curb and Gutter	679	\$100.00	\$67,900			
	Subtotals			\$307,860	\$369,432	\$0	\$369,432
D1	CVS Pharmacy Site	5,790	\$20.00	\$115,800			
	Curb and Gutter	206	\$100.00	\$20,600			
	Subtotals			\$136,400	\$163,680	\$0	\$163,680
F1	Northeast / 14th Frontage	7,351	\$15.00	\$110,265			
	Curb and Gutter	280	\$100.00	\$28,000			
	Subtotals			\$138,265	\$165,918	\$0	\$165,918
Subtotal		26,304	\$22.15	\$582,525	\$699,030	\$0	\$699,030

Total Curbside Improvements

Curbside Total		154,893	\$27.90	\$4,322,015	\$5,186,418	\$1,548,954	\$3,637,464
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Streetside Improvements - 14th Street NW

Phasing / Coordination I.D.	Construction Zone	Construction Area	Unit Cost*	Construction Costs	Construction Costs with 20% Contingency	Projected Developer Costs	Potential Funding Gap / TIF Contribution
		SF.	\$/SF.	(See Note 1)	\$\$\$	(See Note 2)	(See Note 3)
					\$\$\$	%	%
G1	14th St. / Monroe Ave.						
	Crosswalks Only	1,509	\$20.00	\$30,180			
	Subtotals			\$30,180	\$36,216	\$0	\$36,216
G2	14th St. / Park / Kenyon						
	Plaza Extension thru Intersection						
	14th St. R.O.W.	22,870	\$16.00	\$365,920			
	Park Ave. NW R.O.W.	700	\$16.00	\$11,200			
	Park Ave. NE R.O.W.	2,690	\$16.00	\$43,040			
	Kenyon Ave. NE R.O.W.	2,850	\$16.00	\$45,600			
	Subtotals			\$465,760	\$558,912	\$0	\$558,912
G3	14th St. / Midblock	12,039	\$16.00	\$192,624			
	Subtotals			\$192,624	\$231,149	\$0	\$231,149
G4	14th St. / Irving Intersection						
	Plaza Extension thru Intersection						
	14th St. R.O.W.	13,750	\$16.00	\$220,000			
	Irving St. West R.O.W.	3,713	\$16.00	\$59,408			
	Irving St. East R.O.W.	1,038	\$16.00	\$16,608			
	Subtotals			\$296,016	\$355,219	\$0	\$355,219
Streetside Total		61,159	\$16.10	\$984,580	\$1,181,496	\$0	\$1,181,496

Public Realm Improvements - 14th St NW

Public Realm Improvements	Curbside and Streetside Improvements	216,052	\$24.56	\$5,306,595			
	20% Contingency	216,052	\$4.91	\$1,061,319			
	Totals	216,052	\$29.47		\$6,367,914	\$1,548,954	\$4,818,960

Note 1: Refer to Transportation Plan for costs related to signalization, lane striping and other related transportation improvements.

Note 2: Under current District requirements, the developer is responsible for replacing 'public realm' curbside improvements to the existing conditions prior to construction of the private sector project. This budget estimate projects that replacement costs are approximately 50% of final costs. The additional costs include enhanced paving, trees and furnishings.

Note 3: In addition to TIF contributions, potential funding sources include the DC Capital Improvements Program, ad hoc General Fund appropriations, and DC Commission on the Arts and Humanities grants.

Implementation

Preliminary Construction Budget Estimates for Public Realm Elements

The construction costs for public realm improvements within the 'core area' have been developed in consultation with the DC Office of Planning, the District Department of Transportation and the Department of Public Works. They are based upon relevant construction costs for similar public right-of-way projects proximate to the Central Downtown Business District since 2000.

Two different methodologies were used to develop this budget estimate. The first determines and extrapolates unit costs per linear foot of right-of-way for typical 15 ft. and 20 ft. wide sidewalk zones. The second develops more detailed cost estimates for sidewalk zones and the Civic Plaza, including cost allocations for all anticipated elements within the public realm.

Both estimating methodologies resulted in a range of construction costs for public improvements within the 'core area' of approximately \$6.0-\$6.4 million. Consistent with conventional practice for costs projected at this stage of project development, these estimates include a 20% design/estimating contingency.

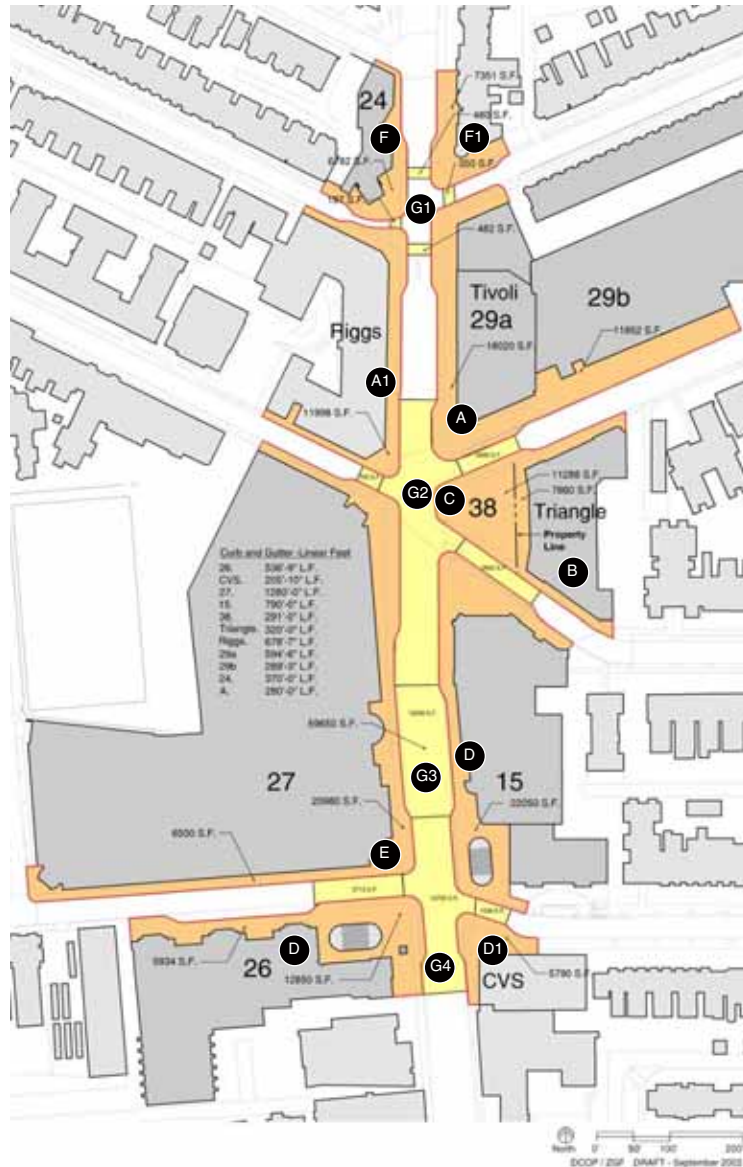
These construction costs are documented in the table to the left, which itemizes the public right-of-way costs in relation to each adjoining development parcel, and are based upon area calculations for the right-of-way, according to and documented on the adjacent Plan illustration.

Total project costs should be budgeted upon existing standard DC DDOT and DPW practices. Total project costs are estimated at \$7.2 million to \$8 million, 20 - 25% higher than base construction costs.

Funding and Financing Strategy

A coordinated funding and financing strategy must be agreed upon by the Office of the Deputy Mayor for Economic Development, the Office of Planning, the Department of Transportation, NCRC, WMATA, and the private development principals. This strategy should include, and leverage:

- coordination of public realm improvements with new development,
- coordination and phasing with planned and future capital improvements,
- creation of local improvement district and use of tax increment financing for infrastructure and public realm improvements,
- WMATA transit area improvement funds,
- additional capital allocations from the District of Columbia,
- coordination of DC Commission of Arts & Humanities artscape improvements.



Site Plan - Preliminary Construction Budget Estimate

Phasing and Coordination Strategy for Public Realm Improvements

Public improvements in the 'core area' should coincide with the construction of each development parcel. As a result, the construction sequence of public improvements depends on the timing and development of the private projects.

- A** The redevelopment of the Tivoli Theatre (RLA Parcel 29) began in July 2003 and should continue until Spring 2005. The related public improvements along the east frontage of 14th Street and the north side of Park Road should be completed in conjunction with these projects

Redevelopment of the public realm on the east side of 14th Street can occur without similar immediate improvements on the west side. However, both sides should be completed in conjunction with reconfiguration of the intersection at 14th St. and Park Road as well as planned improvement for 14th St. and Monroe St. The proposed reconfiguration of the Monroe Street – 14th Street intersection should take place at the conclusion of the improvements described above. Related curbside improvements to the north of this intersection should follow shortly thereafter, and in conjunction with the Dance Institute project on Parcel 24.

- F**
- K** The construction of improvements along Park Road is currently scheduled within the ongoing Capital Improvements Program, administered under DDOT, for Fall 2003. Improvements along the south side of Park Road should be completed at the same time as the DC/USA project (RLA Parcels 27).

- B** The construction of the improvements to the east of 14th Street along the south side of Park Road and the north side of Kenyon Street should be made as Triangle II Development Partners completes its project on the Metro parcel adjacent to RLA Parcel 38. Construction of the Civic Plaza should occur in coordination with this development. Current plans call for the Triangle II project development to begin by Spring, 2004 with completion in Fall-Winter, 2005-2006.
- C**

- D** Improvements around the Metro Station plazas should be constructed as one public project and scheduled concurrently with development by Columbia Heights Ventures (RLA Parcels 15 and 26). Columbia Heights Ventures development projects are expected to start in Spring, 2004 and be completed by early in Fall-Winter, 2005-2006.

Public improvements for both the intersection at Park/Kenyon/14th Street and Irving/14th Street could fall under the same construction contract, and be staged in sequence from north to south in late Winter 2005-2006. Alternatively, the contract for these two public improvement projects could be let together with the Civic Plaza to ensure an efficient project management and coordinated traffic management.

- E** Construction of curbside public improvements adjacent to the DC/USA project (RLA Parcel 27) should be coordinated with construction of that project. DC/USA is scheduled to start construction in Fall 2004. Public and private improvements should be complete by Fall 2006.

Corresponding curbside improvements on the southeast corner of the Irving/14th Street intersection, adjacent to the CVS Pharmacy project should be completed concurrently with the intersection improvements.

Public realm improvements, within the 'core area', could therefore be completed within the next three years, by Fall 2006.

- H** Public realm improvements outside of the 'core area' should be made in relation to other public and private development projects. The Bell-Lincoln Multi-Cultural School is slated for completion by Summer 2005. Public improvements adjacent to the school, including 16th Street, the closure of 15th Street at Irving Street, and Irving Street between 16th Street and 14th Street, should be coordinated with the school reconstruction, the DC/USA project and the Metro Plazas.

- J** If schedules are coordinated with 'core area' improvements, then the substantial construction of Irving Street improvements could be complete in Fall-Winter 2005-06.

- A** The curbside improvements around the Tivoli Theatre block along Monroe St. and Holmead Place should be completed in relation to the multi-family housing project construction.

The remainder of public improvements along 14th Street and throughout the neighborhood will take place according to the priorities for projects under the DC Capital Improvements Program – and in conjunction with related private development projects. Special capital improvement such as public park improvements at 14th Street and Girard Street should also be used to implement the Public Realm Framework.

The improvements should be prioritized as follows:

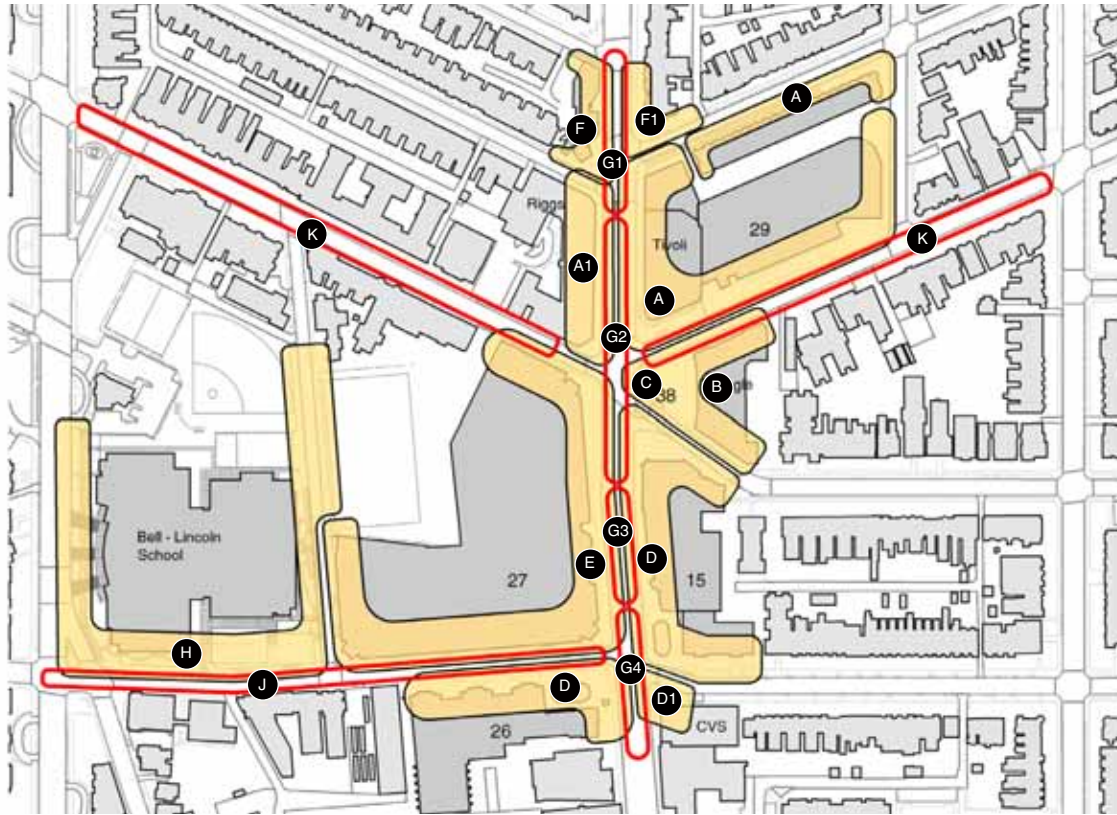
1. 14th Street - south and north of the core area
2. Gateway improvements along 13th Street NW
3. Gateway improvements along 16th Street NW

Coordination of R.O.W. Improvements with DDOT Traffic and Parking Management Plan

The District must adopt the Columbia Heights Traffic and Parking Management Plan (TPMP) to allow for simultaneous construction of public and private improvements as outlined above.

- G** Related design work for right-of-way construction documents must proceed within the next six months to allow for completion by Spring 2005 and construction completion by Fall 2006. This will allow for simultaneous coordination of all public and private development along 14th Street in Columbia Heights.

Public Realm Coordination Diagram



Public Realm Phasing

Projected Construction Schedule

	Phasing Diagram I.D.	RLA Parcels	Project Name	2003				2004				2005				2006				2007			
				Wi	Sp	Su	Fa	Wi	Sp	Su	Fa	Wi	Sp	Su	Fa	Wi	Sp	Su	Fa	Wi	Sp	Su	Fa
Curbside Improvements Development Projects																							
	A	29	Tivoli Theater																				
	B	Metro	Triangle II																				
	C	38	Civic Plaza																				
	D	15	Donatelli & Klein																				
		26	Donatelli & Klein																				
	E	27	DC/USA																				
	F	24	Dance Institute																				
Curbside Improvements - Adjacent to Existing Development																							
	A1		Riggs																				
	D1		CVS																				
	F1		NE 14th St																				
Streetside Improvements - 14th Street NW																							
	G1		14th / Monroe																				
	G2		14th / Park / Kenyon																				
	G3		14th Midblock																				
	G4		14th / Irving																				
Improvements Adjacent to Core Area																							
	H		Bell Lincoln School																				
	J		Irving St																				
	K		Park Road																				

Projected Building Construction Schedule
 Potential Public Realm Construction Period

Public Realm Management

Generally, the regular management and maintenance of curbside improvements within the 'public realm' is the responsibility of abutting private property owners. In complement, the Public Realm Framework recommends the formation of a Business Improvement District to oversee the management and maintenance of public improvements within the 14th Street 'core area' and improvements funded under the proposed Local Improvement District. This organization should evolve from current community associations and established Columbia Heights institutional interests and business community organizations such as CHAMPS, the Columbia Heights Development Task Force and a proposed Main Street organization.

Responsibilities of this body must conform to established District policy and tailored to the proposed physical improvements of this Framework. Responsibility for management and maintenance of the Civic Plaza should fall under the auspices of this organization while ownership of the Plaza shall be retained by the District of Columbia. The stewardship of this Plaza should extend to the creation and management of public-authorized programs for special uses and activities, including the Columbia Heights Farmers Market and a variety of cultural and entertainment programs which should be planned and staged throughout the year.

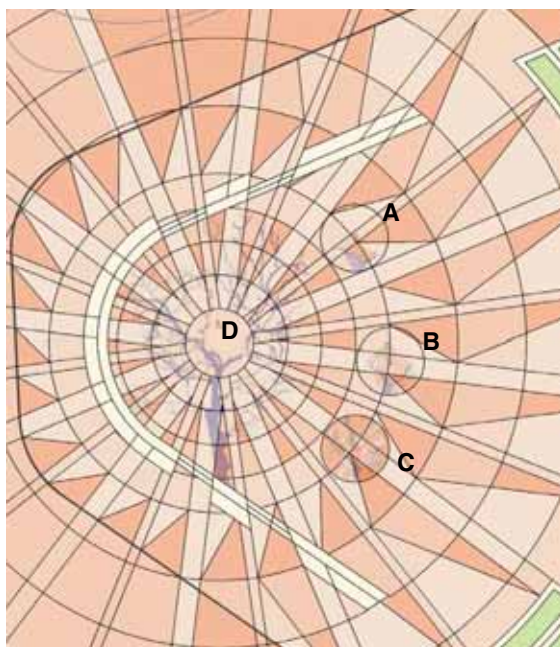
Appendices



- A. Public Art Proposals
- B. Parcel 27 Setback Alternatives
- C. Existing Street Assessments
- D. Schedule of Events

A p p e n d i x A

**Public
Art
Proposals**



Plaza

The fountain will be located in the areas of the watershed illustration



D Potomac and Anacostia Watershed



A L'Enfant Plan - 1791



B McMillan Plan - 1901



C Geological Survey Current

PUBLIC ART PROPOSALS for COLUMBIA HEIGHTS

Jann Rosen-Queralt

Plaza Paving Pattern for Fountain

The Potomac Watershed Basin is a valuable resource to the residents of Columbia Heights. This proposal is to integrate a Map of the Watershed within the central 30-foot diameter circle of paving at the center of the Civic Plaza. Mapping its form as a design overlay on the fountain area of the plaza will focus attention on the Potomac watershed and increase people's awareness of their connection to water in their environment. The map is a beautiful decorative element creating channels in the fountain floor that correspond to the watershed as it moves through the basin. In specific places, parts of the mapped watershed will extend across the Plaza into sidewalk areas. Additionally, three other related maps will enhance the Plaza. These will focus on the Potomac Watershed as it has changed: beginning with a map of L'Enfant's original DC Survey Plan (1791); a map illustrating the MacMillan Plan for DC (1901); and lastly a contemporary map of the District and Watershed from the current year. The Columbia Heights neighborhood is located on each of the maps, to facilitate public understanding of its location within the larger context of the District and the Potomac Watershed.

Mapping of the watershed serves as an ecological educational tool, which reveals information about topography, urban density, and developed land use. Extensions of this idea would be: (1) to capture and direct water runoff which is not re-circulated into the fountain back into landscaping, designing cisterns underneath the plaza for a storage source for landscape irrigation for the Plaza; (2) to color code or mark with a symbol the storm drains along 14th Street and other major thoroughfares in the community, so that residents become more aware of water runoff and its importance; (3) to use native ornamental trees to provide shaded areas around the Plaza; (4) to design drinking fountains on the Plaza so that water, which is not consumed is used for irrigation of the landscaped areas throughout the Plaza.

Community Portraits and Masks

Transformation is integral to contemporary cultural practices of the Columbia Heights community. People are working to better their lives and contribute to their neighborhood. Transitions are being made that involve rituals which celebrate and commemorate public and private life. Examples include birth, death, coming of age, marriage, coronations, or seasonal celebration, which traditionally use 'Masks' that possess spiritual, political, and historical keys to our orientation in the world. It is for this reason that I propose to create a series of images that reveal the connection between people living in Columbia Heights and Masks from their cultural heritages. The ephemeral/evolving quality of transformation is a key to this concept.

The subject matter of the images would represent (1) individual residents from Columbia Heights; (2) groups of residents; (3) ethnographic masks; and (4) ethnographic festivals. These images would be printed on 3m Scotch vinyl that is adhered to square aluminum tubing slipped into an acrylic sleeve so that it is protected from being scratched. The tubing would be put together to form 5' x 8' or 4 x 10' panels that appear to 'flip' or transpose as a viewer walks by. These would be placed in Columbia Heights along 14th Street creating a connection between the Metro Station and Civic Plaza areas, or in areas designated as gateways or thresholds to the community.

Community Wishes

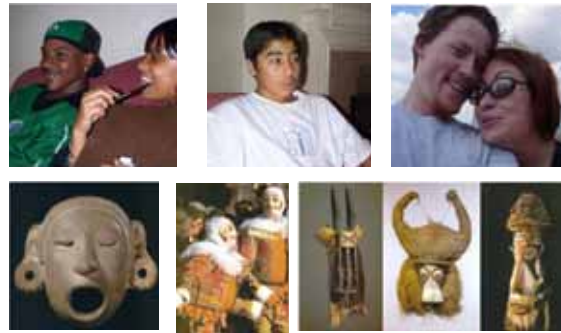
We live in a society where individuality is an important aspiration. The idea that we are individuals as well as members of a greater whole, community is often ignored until a time of need arises. I believe that if there were a way to encourage an awareness of individual similarities, we would have a more cohesive urban environment. An environment based on likeness rather than difference is a goal to strive for. The public recognition of individual wishes and desires is a means to this end.

I have begun this investigation with the youth at the Multicultural Services Center. Four questions were asked. What are your dreams? What are those of a person you admire, such as a parent, guardian, or mentor? What do you desire for the community of Columbia Heights? What do you wish for the world? The answers indicate that a citizen's relationship with his/her community is within reach.

Containers to place wishes in would be a part of the structures in the mask portrait project. Anyone could write their thoughts and put them in the containers. Youth organized by the Multi-Services Center could contact community members of diverse ages, social, economic, and political backgrounds to obtain people's wishes/dreams/desires from all corners of the neighborhood. During the hours of the farmers market, volunteers could gather wishes from the containers and read them. The reading could become a weekly ritual. If embraced by a community center, school, or library it could become an extension of story time when young people are read to aloud or when older children mentor younger ones. This would build creativity, self-expression and self-esteem along with a commitment to community service.

The Columbia Heights Chair

Another way to add character to the Plaza is through the addition of a unique stackable Chair that is inexpensive to produce. It would be light/whimsical/modern in a bright color scheme. This could help to unify the furnishings of the Plaza and could work if (1) all businesses adjacent to the plaza can be convinced to use the same chair and (2) there is a strategy for property security, discouraging theft. The Chairs could be available for daily rental through a form of security exchange to ensure the return of the chairs.



Community Portraits and Masks - Image Examples: Residents, Ethnographic Masks and Festivals



Community Portraits and Masks: Panel Examples and Details



The Columbia Heights Chair



Example of mosaic patterns at seating wall of fountain



Mosaic at seating wall of fountain and bollards



Examples of potential mosaic patterns which may be used to represent the numerous cultures in the neighborhood

PUBLIC ART PROPOSALS for COLUMBIA HEIGHTS

Steven Weitzman
of Creative Design Resolutions, Inc. "CDR"

Design Objectives

Recognizing the important roles of the Columbia Heights Neighborhood and the newly proposed Civic Plaza, as a hub in the community, and a linchpin in the fabric of the surrounding communities, CDR's objectives are to:

- Celebrate the diverse cultural community;
- Create an environment in which the Plaza can serve as a focal point for the continual revitalization of The Columbia Heights Neighborhood;
- Improve the commuter and pedestrian experience along the 14th Street Corridor from the Metro stops past the Tivoli Theater.

Proposal Overview

The proposals for the Columbia Heights Neighborhood begin at the Metro Station, continue down both sides of 14th street, encompass the sidewalk in front of the Tivoli Theatre and highlight the Civic Plaza area.

The incorporation of mosaic treatments on the risers and other vertical faces of the planned curvilinear seating walls surrounding the Plaza will energize and activate this area. These treatments will support the project's theme and will introduce playful, vibrant color into this area. The installation of the mosaics will create a great opportunity to have the residents of Columbia Heights participate in the renovation and revitalization of their neighborhood. Mosaic installation is a fun and educational activity that will be enjoyed by residents of all ages. This opportunity for community participation will generate excitement and interest in Columbia Heights and will, in turn, make the residents feel connected to their neighborhood.

Lighting is a key element in any public space. This proposal offers inventive and artistic solutions to address the lighting needs for The Columbia Heights core area. The installation of a series of "Metro Lights" that begin at the Metro Stations and line 14th Street past the Plaza is a simple, yet elegant, way to illuminate the sidewalk. These lights would tie the Plaza to the two Metro Stations that anchor the Project. The lights would be timed to respond to the arrival and departure of the Metro Trains. This lighting effect will also include a series of bollards that rim the West corner of the Plaza as further protection for the pedestrians. The Civic Plaza is a focal point for the project where people will gather. To emphasize this area as a hub, this proposal includes specialty street lights that would define the space and draw people in. These street lights are inspired by the structure of flowers indigenous to the various countries of origin of the Columbia Heights community. The flower lights would provide shade to pedestrians during the day and would illuminate the Plaza, inviting the residents of Columbia Heights into the area at night.

This proposal also includes translucent lenticular panels which could be incorporated into the facade of the DC-USA building. Thematically, the lenticular design will take imagery from the community wall mural along 14th Street, as well as other images that the team/community would like to include.

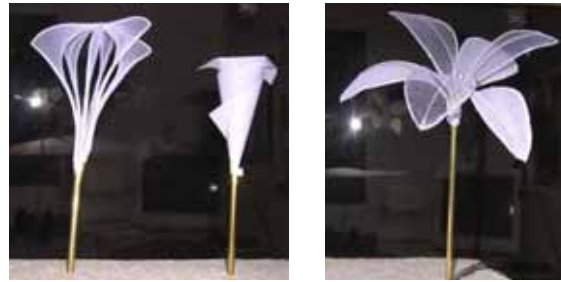
Design Theme

Working in concert with local community members and artisans, CDR will create an exciting mosaic treatment that will charge the space with color and encourage interactivity. The experience created by incorporating mosaic into the vertical surfaces within the planned design will engage pedestrians and will enhance the commuter's experience as well. The theme that unifies CDR's proposed designs is loosely based on the colors and shapes created by the movement of a kaleidoscope. The kaleidoscope theme also inspired shapes of certain indigenous flowers found in the countries of origin of the Columbia Heights residents. The kaleidoscope, with its array of different elements that come together to create many lovely designs, represents the beauty of a diverse neighborhood like Columbia Heights.

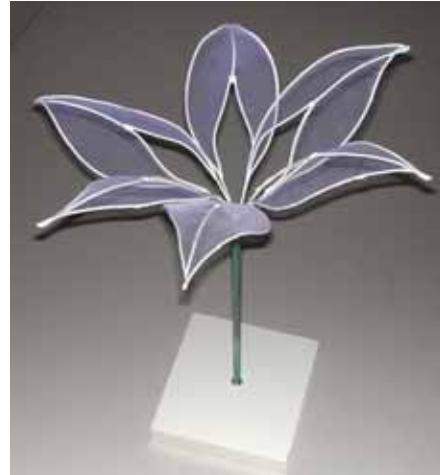
Design Elements

Our design solutions for the 14th Street Corridor leading to and from the Metro Station and the Tivoli Theater contain the following elements:

- Bollard design with relay controlled lights placed at 20' intervals on both the east and west side of 14th Street in the furnishing zone of the sidewalk and along the plaza perimeter;
- Mosaic treatment on the wall bordering 14th Street and on the vertical walls of the lawn areas;
- Tension-fabric structure design for five to nine specialty light standards for the Civic Plaza and Metro Plaza; and
- Lenticular wall panels incorporated into the facades of the DC USA project along 14th Street.



Light and Shade Canopies for the Core Area - Study Models



Light and Shade Canopies for the Core Area - Study Models



Potential mural images integrated with the architecture of DC-USA

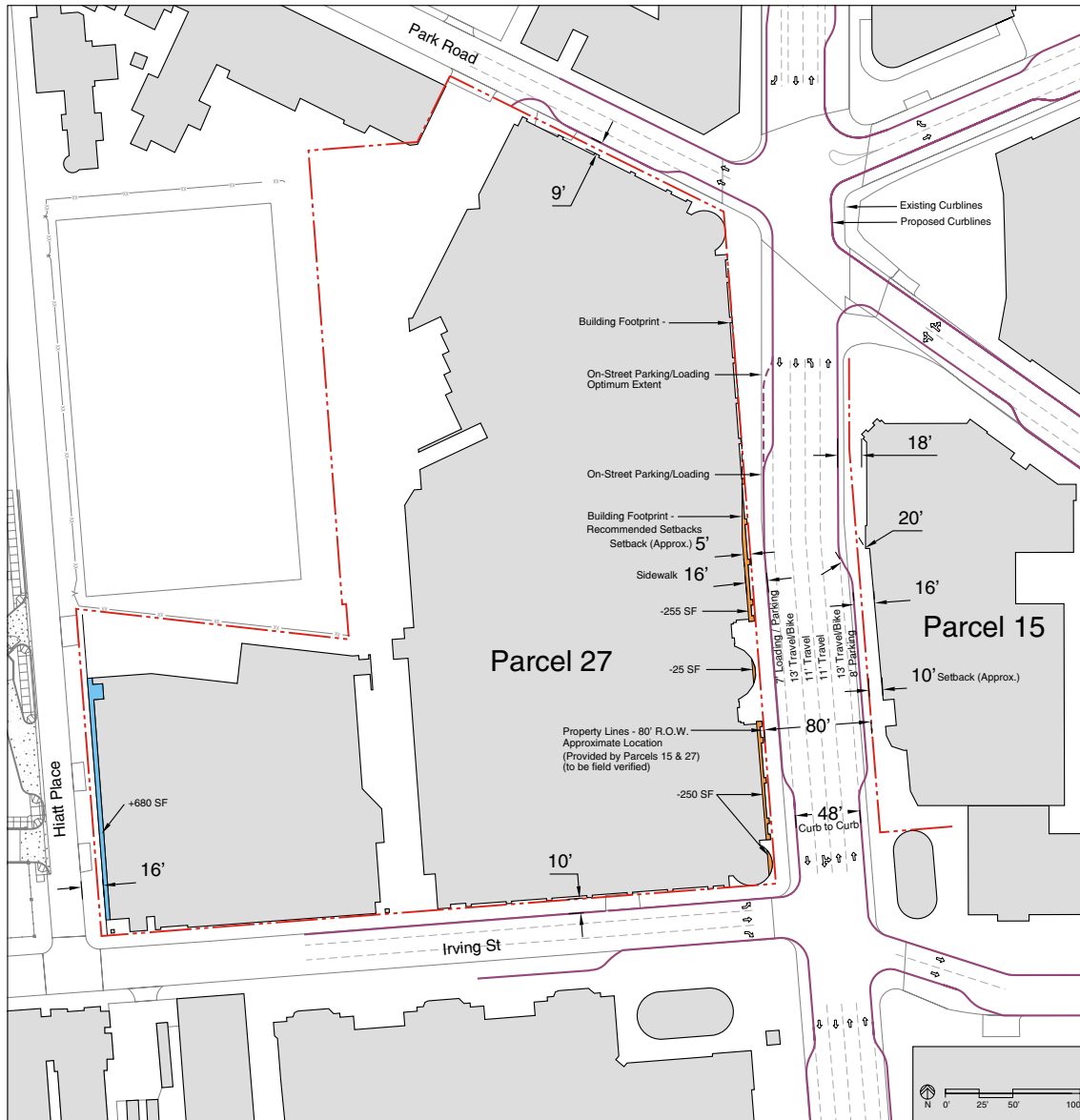
A p p e n d i x B

Parcel 27 Setback Alternatives

PARCEL 27 SETBACK RECOMMENDATIONS

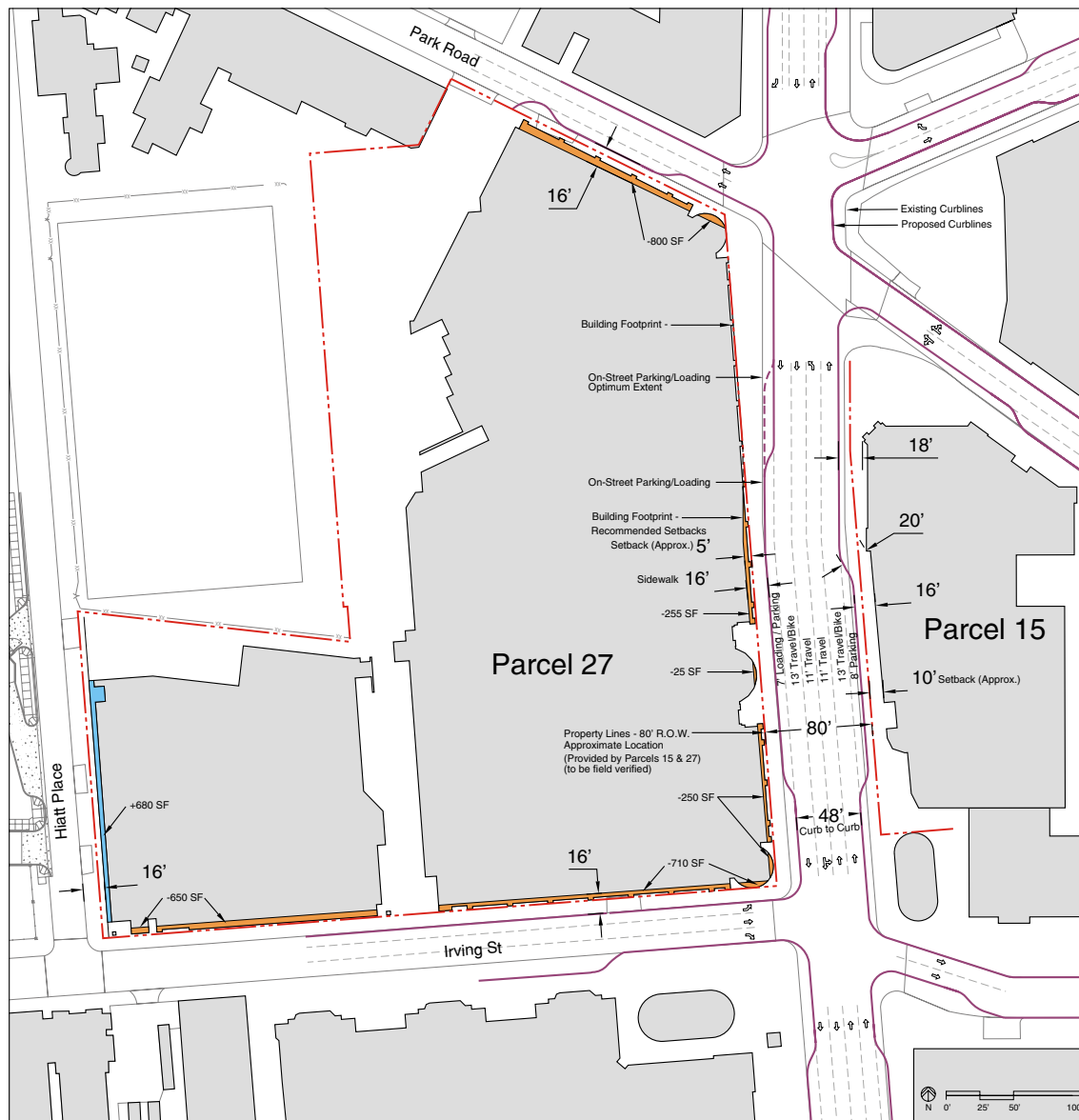
The diagrams below illustrate the recommended setbacks and property impacts for Parcel 27:

- Alternative 1 illustrates a 5' setback along 14th Street to provide the recommended minimum 16' wide sidewalk. The property impact at ground level is approximately 530 square feet.
- Alternative 2 illustrates the setbacks required to provide the recommended minimum 16' sidewalks along 14th Street NW, Park Road NW and Irving Street NW. The property impact at ground level is approximately 2690 square feet.
- Both alternatives illustrate approximately 680 square feet of property along Hiatt Place that exceeds providing the recommended minimum 16' wide sidewalk.



Plan Alternative 1

Note: Property line information to be verified



Plan Alternative 2

Note: Property line information to be verified

A p p e n d i x C

Existing Street Assessments

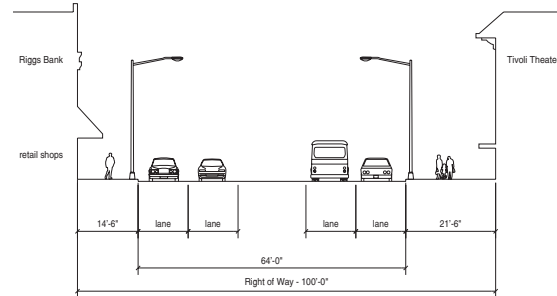


Key Plan

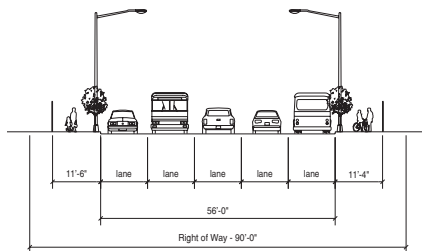
14th Street NW Commercial Corridor Primary Street

Summary of Transportation Plan Preliminary Findings:

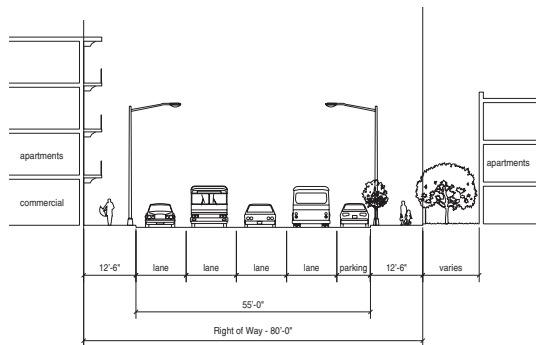
- Significant congestion at 14th/Kenyon/Park intersection
- Highest volume of commuter traffic is along 14th Street
- Bicycle lanes not continuous on 14th Street
- High pedestrian volume on 14th Street - identified as potential safety concern
- Bus shelters are needed on 14th Street



1 14th Street between Park Road and Monroe Typical Section and Plan



2 14th Street between Kenyon and Irving Typical Section and Plan



3 14th Street between Irving and Columbia Road Typical Section and Plan



A Existing Giant Store - Potential Redevelopment Site



B Mixed-Use character and scale



C Pedestrian barriers on 14th St NW



D West side of 14th St NW



E Existing sidewalk conditions along east side of 14th St NW



F High-density residential along 14th St NW



G Metro entrance on southwest corner of 14th and Irving

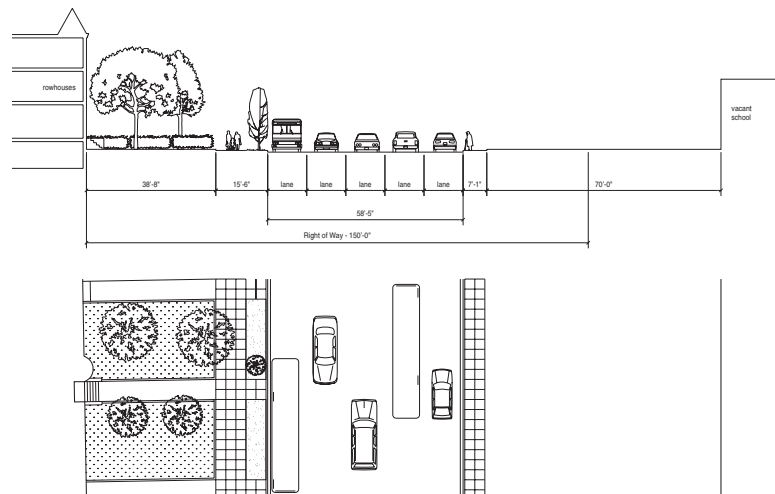


Key Plan

16th Street NW District Connector

Summary of Transportation Plan Preliminary Findings:

- Significant congestion at 16th Street / Park and 16th Street / Columbia
- High proportion of commuter traffic on 16th Street
- Significant number of pedestrian accidents at 16th Street / Irving and confusing intersection at 16th Street / Mt Pleasant / Harvard. Area identified as potential safety concern



1 16th Street between Park Road and Irving
Typical Section and Plan



A Existing sidewalk condition along 16th St NW



B Existing playground



C Entry into neighborhood from 16th St NW



D Fence along school property



E View north to high-density residential

F Neighborhood entry at Columbia Road and Harvard St NW

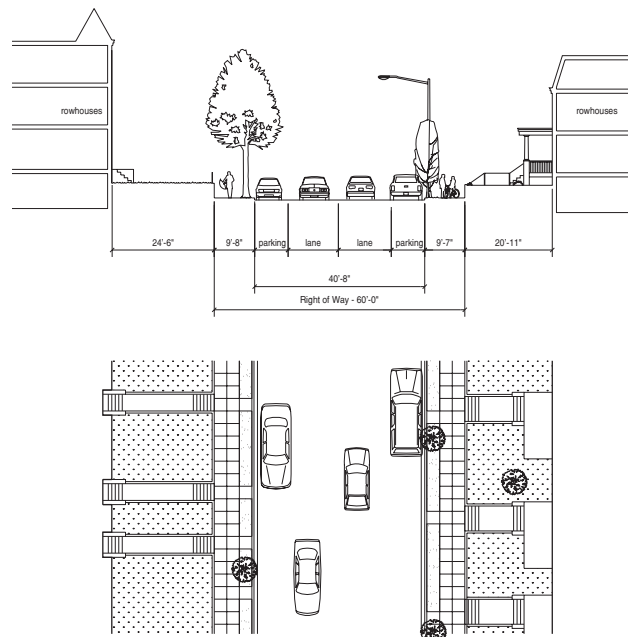


Key Plan

13th Street NW Neighborhood Connector

Summary of Transportation Plan Preliminary Findings:

- Traffic volumes are highest north of Monroe and between Irving and Columbia Road
- Highest number of automobile crashes along 13th Street are at intersection with Columbia Road
- Pedestrian accidents along 13th Street are highest at Columbia Road intersection.



1 13th Street between Irving and Columbia Road
Typical Section and Plan



A Existing sidewalk condition



B Pedestrian entry from east of 13th St NW



C Existing tree well



D Sidewalk adjacent to Tubman School



E Residential character and scale



Key Plan

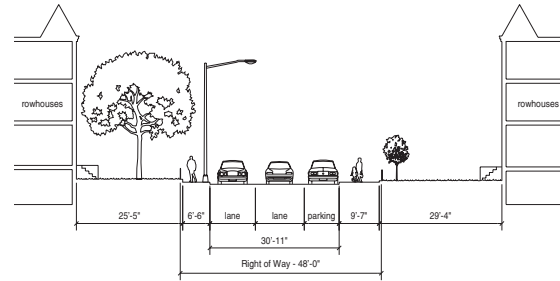
Park Road NW Mixed-Use Secondary Street

Summary of Transportation Plan Preliminary Findings:

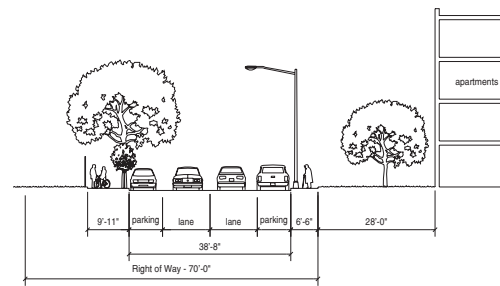
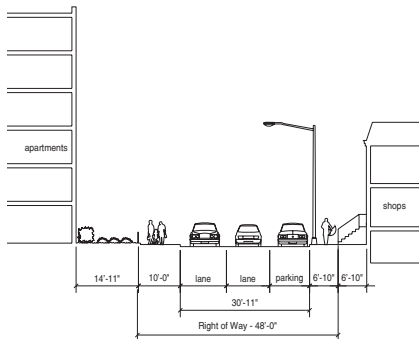
- Significant congestion at Park/Kenyon/14th Street intersection
- Highest number of automobile crashes are at 16th Street and 14th Street intersections
- Highest pedestrian volume in neighborhood at Park/Kenyon/14th intersection

Citizen concerns:

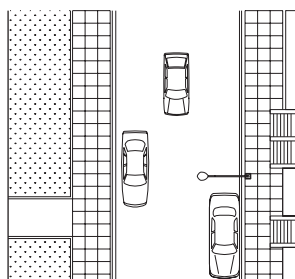
- High pedestrian traffic and lack of trash cans results in littering



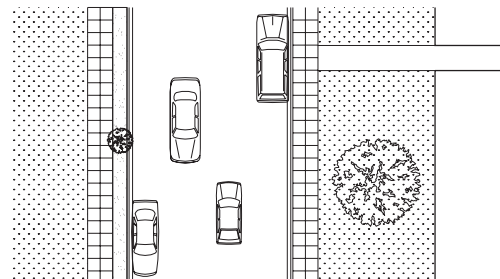
1 Park Road between 16th and Hiatt Typical Section and Plan



2 Park Road between Hiatt and 14th Typical Section and Plan



3 Park Road between 14th and Holmead Pl Typical Section and Plan





A Existing sidewalk adjacent to proposed plaza site



B Existing sidewalk adjacent to Tivoli



C Entry to neighborhood from 16th St NW



D Mixed-use character and scale west of 14th St NW



E Mixed-use character and scale east of 14th St NW

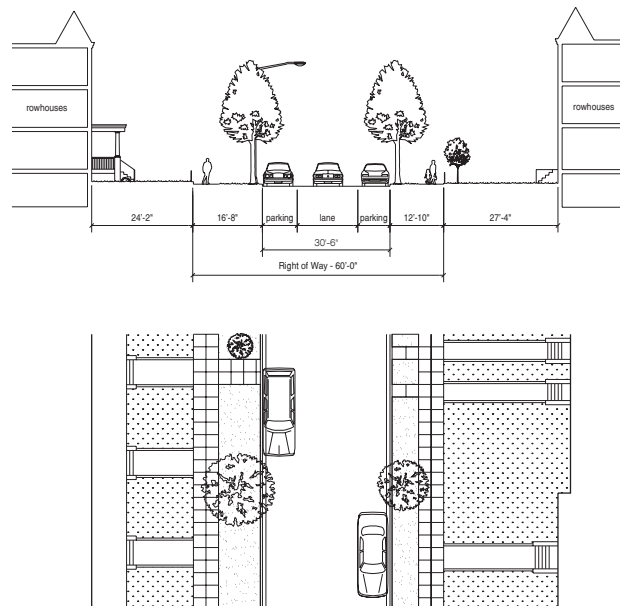


Key Plan

Kenyon Street NW Mixed-Use Secondary Street

Summary of Transportation Plan Preliminary Findings:

- Significant congestion at Park/Kenyon/14th Street intersection
- Highest number of automobile crashes are at 14th Street intersection
- Highest pedestrian volume in neighborhood at Park/Kenyon/14th intersection



1 Kenyon between 14th and 13th
Typical Section and Plan



A Existing sidewalk condition



B Existing sidewalk condition



C Existing tree canopy over street



D Existing sidewalk adjacent to development parcel



E Residential character and scale



Key Plan

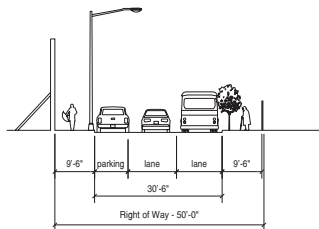
Irving Street NW Mixed-Use Secondary Street

Summary of Transportation Plan Preliminary Findings:

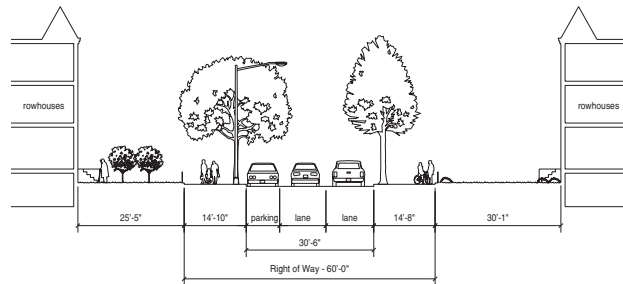
- Highest number of automobile crashes along Irving Street are at 16th Street intersection
- High pedestrian volume and highest pedestrian crashes at 16th Street intersection. Intersection identified as potential safety concern

Citizen Concerns:

- Irving provides primary pedestrian connection between Metro and Mt. Pleasant neighborhood and should be improved.



1 Irving between 15th and 14th
Typical Section and Plan



2 Irving between 14th and 13th
Typical Section and Plan



A Existing sidewalk adjacent to residential properties



B Existing sidewalk near 14th St NW



C Existing tree wells



D Entry into neighborhood at Lincoln School



E Proposed DC-USA site

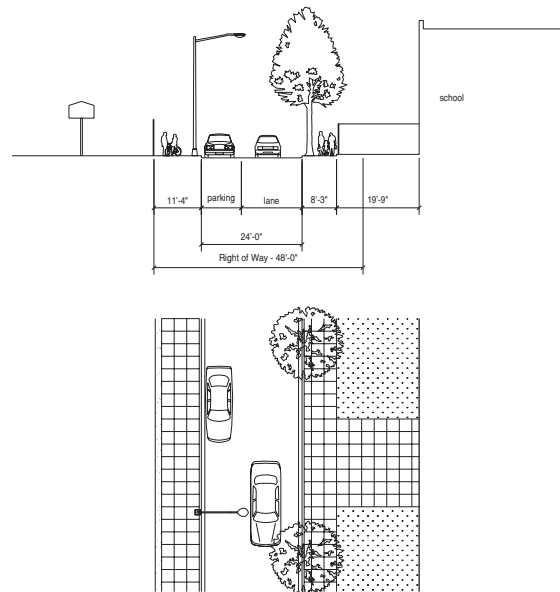


Key Plan

Hiatt Place NW Mixed-Use Secondary Street

Citizen Concerns:

- Lighting and safety should be enhanced



1 Hiatt between Park Road and Irving
Typical Section and Plan



A Existing sidewalk adjacent to proposed DC-USA site



B Entry to Bell School



C Residential character



D Existing street tree canopy



E Sidewalk and fence adjacent to Bell School

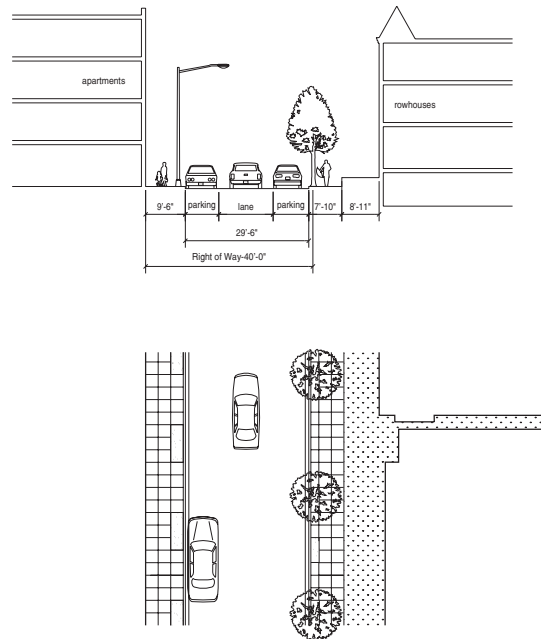


Key Plan

15th Street NW Mixed-Use Secondary Street

Summary of Transportation Plan Preliminary Findings:

- High number of automobile crashes at intersection with 16th Street and Irving Street.



1 15th between Irving and Columbia Road
Typical Section and Plan



A High-density residential



B Sidewalk adjacent to high-density residential



C Neighborhood entry at intersection of 15th, 16th, and Irving Sts.



D Existing sidewalk condition on east side of 15th St NW



E View north of street illustrating fixtures, tree canopy and building scale

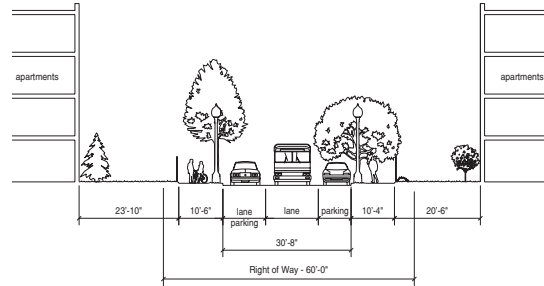


Key Plan

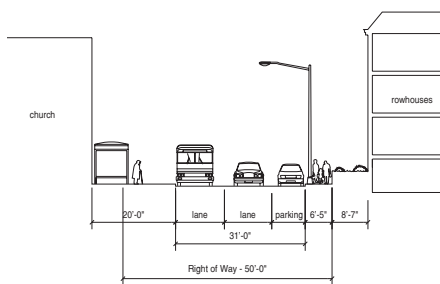
Columbia Road NW Mixed-Use / Residential Secondary Street

Summary of Transportation Plan Preliminary Findings:

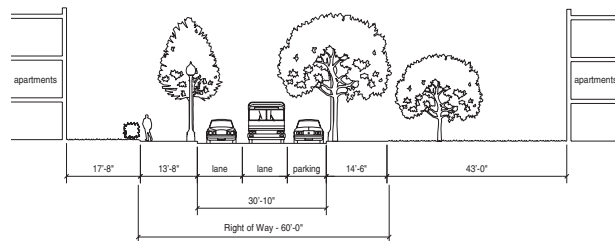
- Significant congestion at 16th Street intersection
- Highest number of automobile and pedestrian crashes on Columbia Road are at 13th Street intersection
- Intersections with 16th and 14th Streets identified as potential safety concerns



2 Columbia Road between 15th and 14th
Typical Section and Plan



1 Columbia Road between 16th and 15th
Typical Section and Plan



3 Columbia Road between 14th and 13th
Typical Section and Plan



A Sidewalk character west of 14th St NW



B Sidewalk character east of 14th St NW



C Retail frontage at 14th St NW



D View toward 16th St NW



E Pedestrian entry from west into neighborhood

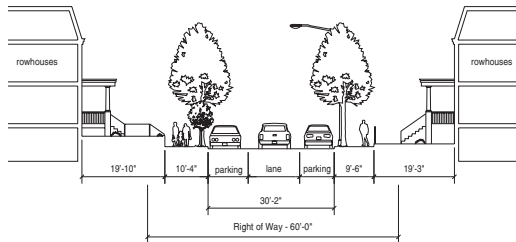


Key Plan

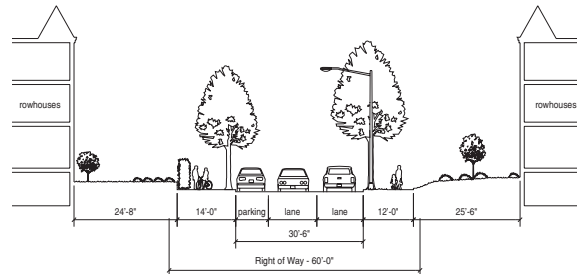
Harvard Street NW Residential Secondary Street Girard Street NW (Similar)

Summary of Transportation Plan Preliminary Findings:

- Highest automobile crash on Harvard are at 16th Street intersection
- Intersection with 16th identified as potential safety concern area



1 Harvard between 15th and 14th
Typical Section and Plan



2 Harvard between 14th and 13th
Typical Section and Plan



A Existing sidewalk condition adjacent to residential property west of 14th St NW



B Sidewalk east of 14th St NW



C Residential character and scale



D Existing sidewalk adjacent to high-density residential



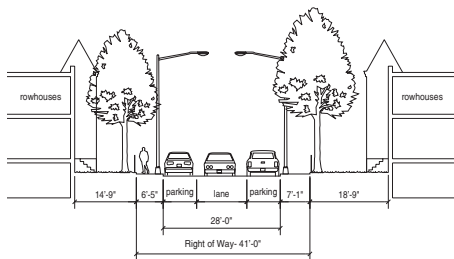
E Future Urban League Headquarters



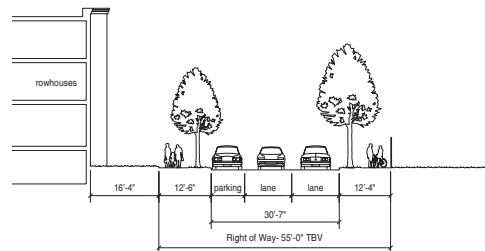
Key Plan

Monroe Street NW Residential Secondary Street Newton Street NW and Holmead Place (similar)

- Summary of Transportation Plan Preliminary Findings:**
- Highest level of congestion on Monroe is at 14th Street intersection



1 Monroe between 16th and 14th
Typical Section and Plan



2 Monroe between 14th and Holmead Pl
Typical Section and Plan



A Existing sidewalk east of 14th St NW



B Existing sidewalk west of 14th St NW



C Personalized landscaping in tree well



D Residential character and tree canopy west of 14th St NW



E Residential character adjacent to Tivol development site

Appendix D

Schedule of Events



Government of the District of Columbia
Anthony A. Williams, Mayor

Schedule of Events

Columbia Heights Public Realm Framework and Neighborhood Transportation Plan

Date/ Time	Event	Location
Wed 03/26/03 6:00–8:00PM	Advisory Committee Meeting #1 <i>Meet consultant team; Roles and Responsibilities; review calendar</i>	DCCH Conference Room 3419 14 th Street, NW
Tues 04/08/03 6:30–8:30PM	NCRC Public Information Meeting <i>Update and progress on redevelopment parcels in Columbia Heights</i>	Bell Multicultural High School
Wed 04/14/03 6:30–8:30PM	Community Kick-Off Meeting <i>Introduction of process; review past planning efforts and priorities</i>	Calvary Multicultural Ctr. 142 Columbia Road, NW
Tues 04/22/03 6:00–8:00 PM	Advisory Committee Meeting #2 <i>Public Realm Priorities focus, preview Workshop#1 agenda</i>	Latin American Youth Center
Tues 05/07/03 6:30–9:00 PM	Community Workshop #1 <i>Establish framework for Public Realm Guidelines</i>	Tubman ES
Wed 05/28/03 6:00–8:00 PM	Advisory Committee Meeting #3 <i>Neighborhood Transportation Plan focus, preview Workshop#2 agenda</i>	Latin American Youth Center
Tues 06/03/03 6:30–9:00 PM	Community Workshop #2 <i>Neighborhood Transportation Plan Analysis and Strategies</i>	Tubman ES
Wed 06/11/03 6:00–9:00 PM	Design Advisory Committee Meeting #1	Zimmer Gunsul Frasca Offices
Tues 06/17/03 6:00–9:00 PM	Design Advisory Committee Meeting #2	Zimmer Gunsul Frasca Offices
Wed 07/01/03 6:00–8:00 PM	Advisory Committee Meeting #4 <i>Review Draft Public Realm Implementation and Draft Transportation Recommendations</i>	Latin American Youth Center
Tues 07/08/03 6:00–9:00 PM	Design Advisory Committee Meeting #3	Zimmer Gunsul Frasca Offices
Sat 07/12/03 7:00–9:00 PM	Community Workshop #3 <i>Draft Implementation Strategy and Draft Transportation Recommendations</i>	Tubman ES
Tues 09/23/03 6:00–9:00 PM	Design Advisory Committee Meeting #4	Zimmer Gunsul Frasca Offices
Wed 10/15/03 7:00–9:00 PM	Final Draft Plan Presentation #1 <i>Neighborhood Transportation Plan</i>	Tubman ES
Mon 10/27/03 7:00–9:00 PM	Final Draft Plan Presentation #2 <i>Public Realm Framework</i>	Tubman ES



Acknowledgements

Acknowledgements

Mayor Anthony A. Williams

Councilmember Jim Graham, *Ward 1*

Deputy Mayor Eric Price

Office of Planning

Andrew Altman, *Director*

Toni Griffin, *Deputy Director*

Mitchell Silver, *Deputy Director*

Michael Downie, *Revitalization Planner - Project Manager*

Karina Ricks, *Revitalization Planner*

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Office of Economic Development

Michael Jasso, *Special Assistant*

Alex Nyhan, *Special Assistant*

National Capital Revitalization Corporation

Ted Carter, *President and C.E.O.*

Simone Goring, *Vice-President of Real Estate Development*

Ruth Uchiyama, *Development Manager*

Arts and Humanities Commission

Sherry Schwechten, *Art in Public Places Manager*

Victoria Reis, *Art in Public Places Consultant*

Advisory Committee (* Also member of Project Design Subcommittee)

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Nigel Gragg

Mack James*

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David Levy*

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